

# TND\_OVERLAY

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City of Conway, Arkansas

**AN ORDINANCE ESTABLISHING THE TRADITIONAL NEIGHBORHOOD DEVELOPMENT OVERLAY  
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## ARTICLE 1. ORDINANCE ADOPTION

### 1.1 AUTHORITY

#### ORDINANCE O-06-\_\_

**AN ORDINANCE ESTABLISHING THE TRADITIONAL NEIGHBORHOOD DEVELOPMENT OVERLAY DISTRICT, PURSUANT TO THE DESIGN OVERLAY AUTHORITY ADOPTED IN ORDINANCE O-99-72 OF THE CITY OF CONWAY, ARKANSAS, DECLARING AN EMERGENCY; AND FOR OTHER PURPOSES:**

**WHEREAS**, the City of Conway desires to encourage the creation of neighborhoods that are compact, pedestrian-oriented and mixed-use; and

**WHEREAS**, the establishment of neighborhoods where ordinary activities of daily living occur within walking distance of most dwellings promotes improved public health, conserves natural resources and allows independence for those who do not drive; and

**WHEREAS**, the formation of thoroughfare networks allow for the dispersion of traffic and reduction of the length of automobile trips; and

**WHEREAS**, it is in furtherance of the public interest and welfare to create neighborhoods where a range of housing types and price levels are provided to accommodate diverse ages and incomes; and

**WHEREAS**, it is also desirable to encourage the creation of neighborhoods where appropriate building densities and land uses are provided within walking distance of transit stops and schools; and

**WHEREAS**, it is in the public interest to create neighborhoods where a range of open space including parks, squares, and playgrounds are distributed within the neighborhood;

**NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF CONWAY, ARKANSAS:**

**SECTION 1:** That the City of Conway, Arkansas hereby approves and adopts by reference the Traditional Neighborhood Development District Ordinance (TND Overlay District) pursuant to the design overlay authority adopted in Ordinance O-99-72, and approved by the Conway Planning Commission, not less than three (3) copies of said ordinance consisting of text and graphics are now filed in the office of the Clerk of the City of Conway, Arkansas, and from the date on which this ordinance shall take effect the provisions thereof shall be established and applied to the following area:

#### A. THE VILLAGE AT HENDRIX LEGAL DESCRIPTION

THAT PORTION OF THE (W 1/2 OF THE NW 1/4 OF SECTION 6, TOWNSHIP 5 NORTH, RANGE 13 WEST AND THE E 1/2 OF THE NE 1/4, SECTION 1, TOWNSHIP 5 NORTH, RANGE 14 WEST; CONWAY, FAULKNER CO., AR.) LYING EAST OF THE EAST RIGHT OF WAY LINE OF HIGHWAY 65B (HARKRIDER ST.). LYING NORTH OF THE NORTH RIGHT OF WAY LINE OF HIGHWAY #266 (SIEBENMORGEN); AND LYING WEST OF THE WEST RIGHT OF WAY OF I-40. MORE PARTICULARLY DESCRIBED AS FOLLOWS: BEGINNING AT THE COMMON CORNER OF THE INTERSECTION OF THE NORTH LINE OF TOWNSHIP 5 NORTH AND THE EAST LINE OF RANGE 14 WEST; THENCE N89°21'42"E A DISTANCE OF 988.98 FEET TO A CONCRETE MONUMENT ON THE WEST RIGHT OF WAY LINE OF I-40; THENCE S00°05'22"E ALONG SAID RIGHT OF WAY LINE A DISTANCE OF 444.19 FEET TO A CONCRETE MONU-

MENT; THENCE S00°01'17"E A DISTANCE OF 162.52 FEET TO A CONCRETE MONUMENT; THENCE S00°15'14"E A DISTANCE OF 254.13 FEET TO A CONCRETE MONUMENT; THENCE S07°23'24"E A DISTANCE OF 414.04 FEET TO A CONCRETE MONUMENT; THENCE S16°45'04"E A DISTANCE OF 250.40 FEET TO A POINT ON THE RIGHT OF WAY AND A POINT OF CURVATURE OF A 4440.00 FOOT RADIUS CURVE TO THE RIGHT HAVING A CHORD BEARING AND DISTANCE OF S75°17'44"W, 1418.60 FEET; THENCE LEAVING SAID RIGHT OF WAY ALONG SAID CURVE A DISTANCE OF 1424.70 FEET TO THE POINT OF TANGENCY, WHICH IS THE POINT OF CURVATURE OF A 436.79 FOOT RADIUS CURVE TO THE LEFT HAVING A CHORD BEARING AND DISTANCE OF S73°58'45"W, 159.33 FEET; THENCE ALONG SAID CURVE A DISTANCE OF 160.23 FEET TO THE POINT OF TANGENCY; THENCE S88°42'59"W, 143.53 FEET TO A POINT; THENCE S01°17'01"E, 237.83 FEET TO A POINT; THENCE S52°24'16"W A DISTANCE OF 221.53 FEET TO A POINT ON THE EAST RIGHT OF WAY LINE OF U.S. HWY 65B (HARKRIDER STREET); THENCE N25°44'57"W ALONG SAID RIGHT OF WAY LINE A DISTANCE OF 991.60 FEET TO THE P.C. OF A CURVE BEING A 1232.41 FOOT RADIUS CURVE TO THE RIGHT HAVING A CHORD BEARING AND DISTANCE OF N12°54'27"W A DISTANCE OF 525.61 FEET, THENCE ALONG SAID CURVE 529.68 FEET TO THE POINT OF TANGENCY; THENCE N00°03'58"W A DISTANCE OF 373.40 FEET TO A POINT; THENCE N89°50'14"E A DISTANCE OF 25.00 FEET TO A POINT; THENCE N00°03'56"W A DISTANCE OF 498.0 FEET TO A POINT AT THE INTERSECTION WITH THE NORTH LINE OF SECTION 1, TOWNSHIP 5 NORTH, RANGE 14 WEST; THENCE N89°50'28"E A DISTANCE OF 1246.95 FEET TO THE POINT OF BEGINNING CONTAINING 93.32 ACRES.

And such other areas that may from time to time be added to this TND Overlay District by the act of the City Council of Conway.

**SECTION 2:** That all ordinances and part of ordinances of a permanent and general nature in effect at the time of adoption of this ordinance, and not included herein, are hereby superseded where they are in conflict with this ordinance.

**SECTION 3:** It being immediately necessary for the preservation of the public peace, health, safety of the City of Conway, Arkansas, and the inhabitants thereof, an emergency is hereby declared to exist, by reason thereof this ordinance shall take effect and be in full force and effect from and after its passage and publication.

PASSED this \_\_\_\_ day of \_\_\_\_ 2006.

**ATTEST:**

**APPROVED:**

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Mayor



**1.2 INTENT**

The Traditional Neighborhood Development Overlay District (or “TND Overlay District”) is established to promote and encourage the creation of neighborhoods:

- 1.2.1 That are compact, pedestrian-oriented and mixed-use; and
- 1.2.2 wherein ordinary activities of daily living occur within walking distance of most dwellings, thereby promoting the public health, conserving natural resources, and allowing independence to those who do not drive; and
- 1.2.3 wherein networks of thoroughfares disperse traffic and reduce the length of automobile trips; and
- 1.2.4 wherein a range of housing types and price levels are provided to accommodate diverse ages and incomes; and
- 1.2.5 wherein appropriate building densities and land uses are provided within walking distance of transit stops and schools; and
- 1.2.6 wherein a range of open space including parks, squares, and playgrounds are distributed.

**1.3 APPLICABILITY**

- 1.3.1 This TND Overlay is intended to provide development regulations that supersede both the Zoning Ordinance and Subdivision Ordinance.
- 1.3.2 Provisions of this TND Overlay are activated by “shall” when required; “should” when recommended; and “may” when optional.
- 1.3.3 The provisions of this TND Overlay Ordinance, when in conflict, shall take precedence over those of other all city ordinances, including but not limited to the existing Zoning Ordinance and the Subdivision Ordinance. All other city ordinances shall continue to be applicable to issues not covered by this TND Overlay except where these ordinances would be in conflict with the Intent in Section 1.2, in which case the conflict shall be resolved in favor of this TND Overlay.
- 1.3.4 Terms used throughout this TND Overlay shall be accorded their commonly accepted meanings or as defined in Articles 1-6 hereof or in the Definitions of Terms (Article 7). In the event of conflicts between these definitions and those of all other city ordinances, those of this TND Overlay shall take precedence.
- 1.3.5 The Definitions of Terms (Article 7) contains regulatory language that is integral to this TND Overlay. Capitalized terms in Articles 1-6 of this TND Overlay may refer to Article 7 Definitions.

**1.4 INCENTIVES**

- 1.4.1 To encourage the creation of walkable, mixed-use neighborhoods, the City Council grants that applications for development within the TND Overlay District may be processed administratively. The Council may also from time to time grant additional incentives to encourage the creation of such neighborhoods.

**ARTICLE 2. PROCESS****2.1 GENERAL INSTRUCTIONS**

- 2.1.1 The Traditional Neighborhood Development Overlay District applies to designated areas. After designation, the property owner(s) within a district or the City may submit a TND Plan, consisting of a Master Plan and Regulating Plan for review and approval by the City Council. Subsequent Preliminary Plat Plans and Preliminary Plat Data shall comply, in general, with the TND Plan.
- 2.1.2 These overlay district standards shall be available as an optional overlay by right. The underlying zoning district(s) also shall remain available by right, until an approved TND plan and plat supersedes the underlying district. These overlay district standards shall be applied in their entirety or not at all. Once the final plat is granted for a specified overlay district, the overlay district standards supersede the underlying district(s).

**2.2 TND OVERLAY DISTRICT REZONING**

- 2.2.1 The establishment of this TND Overlay District ordinance creates a floating overlay zone that can be applied when adopted in a specific location.
- 2.2.2 This TND Overlay District is applied to a specific parcel or group of parcels when the legal description of such parcel(s) is added to Article 1, Section 1 of this Ordinance and shall be considered a rezoning.
- 2.2.3 Any minor changes to the Master Plan or Regulating Plan may be approved by the Planning Director. Any major changes are subject to the review of the City Council. A major change is considered to be any of the following: increasing fifty percent or more of the allocated Transect Zones to zones of higher intensity than the regulating plan; adding land parcels to the area regulated by the TND Overlay District, the Master Plan and the Regulating Plan; or a substantial deviation from the nature and character of the Master Plan.
- 2.2.4 This TND Overlay District Ordinance in no way relieves the obligation for the payment of impact fees as prescribed in the Conway Subdivision Ordinance Article VI Section 12. Impact Fees.

**2.3 TND PLAN SUBMITTAL AND APPROVAL**

- 2.3.2 TND Plan Pre-Application Conference
- To begin the approval process for a TND Plan, the Developer should discuss informally the intent of the proposed TND Plan with the members of the City of Conway Development Review Committee (DRC). The DRC is composed of one representative from each of the regulatory agencies that have jurisdiction over the permitting of a project, including but not limited to the Planning Commission, Planning Department, Fire Department, Conway Corporation, Sanitation Department and Street Department. No fees shall be collected for Pre-Application Conference, its purpose being to familiarize the planning department with the proposed TND Plan and the Developer with the development procedures in Conway's TND Overlay District. The Developer should share sketch plans and data showing existing site conditions and the proposed layout and development of the TND Plan. The Developer may also share the acreage of the tract, proposed land uses, a proposed Bill of Assurance and any unusual site characteristics. At the Pre-Application Conference, the general character of the development will be discussed and items including zoning, utility service, street requirements, flooding and drainage, and other pertinent factors related to the proposed development will be reviewed. The purpose and intent of the Pre-Application Conference allows both parties to identify potential challenges, opportunities and items that need to be addressed.

**2.3.3 TND Plan Submission and Approval**

The following must be submitted for a TND Plan Submission:

- a. A TND Plan, composed of a Master Plan and a Regulating Plan, shall be submitted to the Conway Planning Department for review and approval by the City Council.

Master Plan

The Master Plan is a rendered plan that reflects the character of the overall development. It shall show streets, blocks, general lot sizes, public spaces and may show public buildings or other notable buildings. It shall also show the phasing for the entire tract.

Regulating Plan

The Regulating Plan shall assign Transect Zones to the entire Traditional Neighborhood Development Plan.

- b. A Statement of Intent describing how the TND Plan submitted is consistent with the intent and spirit of the TND Overlay District.
- c. Preliminary Architecture and Landscape Patternbook. This book will include general concepts of proposed building style, shape and design requirements and recommendations for buildings in the TND Overlay District.

**2.3.4 For TND Plan approval, the following must addressed in the TND Plan, Statement of Intent or Patternbook:**

- (1) Plan Phasing
- (2) Thoroughfare network (including rights of way)
- (3) Building Density
- (4) Building and Parking Placement
- (5) Building Height
- (6) Building Function
- (7) Architecture and Landscape
- (8) Screening, Signage and any other requirements

**2.4 PROCEDURE FOR OBTAINING A PLAT WITHIN THE TND OVERLAY DISTRICT****2.4.1 TND Overlay District Preliminary Plat Process**

TND Plan approval by the City Council stands for one year from the date of approval. If the TND preliminary plat is not submitted for approval within the year after the TND plan approval, the TND plan becomes null and void. Review and re-approval shall be required for any new plan, or the same or similar plan, according to the standards and procedures of this ordinance. Following the review and approval of a preliminary or final plat associated with the TND plan, the TND plan shall be valid for the period of the preliminary or final plat's validity. In the case of a phased plan, the TND plan shall be considered complete after final plat approval of the first phase.

The first required step in the Preliminary Plat review process is the submission of an Application for Preliminary Plat Approval. Prior to any development, an application for preliminary plat approval based on the approved TND Plan, must be submitted and approved as follows.

**a. Submission Requirements**

The Application shall be submitted to the Planning Director and shall consist of the following:

- (1) A Letter of Intent.
- (2) Five (5) copies of the Preliminary Plat Plans; and Preliminary Plat Data. The Preliminary Plat Plans and Preliminary Plat Data shall include the items required in Section 2.4.2, Preliminary Plat Submittal Requirements.
- (3) A filing fee of \$150 + \$5 per lot.

**b. Preliminary Approval**

After the Planning Commission has reviewed the Preliminary Plat and taken into account any staff recommendation, the Applicant shall be advised of any required changes and/or additions. The Planning Commission shall approve, approve conditionally, or disapprove the Preliminary Plat within eighty (80) days from the date of receipt thereof or the Preliminary Plat shall be deemed approved unless the developer stipulates in writing to the Planning Commission that additional time is allowed. If disapproved, the Preliminary Plat shall be returned to the developer with a written statement as to the reasons for disapproval.

The Planning Commission's approval of the Preliminary Plat shall be deemed as an expression of approval of the layout submitted on the Preliminary Plat as a guide to the installation of streets, water, drainage, sewer, and other required improvements and utilities, the dedication and reservation of public lands, and to the preparation of the Final Plat. Approval of a Preliminary Plat shall not constitute automatic approval of the Final Plat.

When the Planning Commission finds the Preliminary Plat Plan together with the Preliminary Plat Data meets all the requirements of this Regulation, it shall approve the Plat by placing a Certificate of Preliminary Plat Approval upon the Preliminary Plat. This stamp of Preliminary Plat Approval shall read:

"This Plat has been given Preliminary Plat Approval only and has not been approved for recording as a public record. This Certificate shall expire on \_\_\_\_\_ (date)."

(The certificate shall expire 12 months from date of approval unless extended by the Planning Commission.) Such stamp approval shall bear the signature of the Chairman of the Planning Commission and shall be dated. Six copies of the Preliminary Plat shall be submitted. Five copies shall be distributed and/or retained by the Planning Commission and one copy shall be returned to the developer upon approval.

**c. Authorization to Proceed**

Receipt of an approved or conditionally approved copy of the Preliminary Plat, together with an approved copy of the Improvements Plan shall constitute authorization of the Planning Commission for the Developer to proceed with the preparation of the Final Plat, the installation of improvements, and the staking out of lots and blocks. The Developer, after conditional approval of the Preliminary Plat, shall complete all improvements required under this Regulation.

**d. Expiration of Preliminary Plat Approval**

If at the end of the twelve (12) month date of approval of the Preliminary Plat the Developer submits a written request for extension of the preliminary approval, the Planning Director may grant an extension of up to twelve (12) additional months providing, in the opinion of the Planning Director, sufficient work has been completed with respect to the required improvements on the property.



Without the request for extension, the preliminary plat approval shall automatically expire 12 months from the original date of approval and further development work will require approval of another Preliminary Plat. Further, no more than one extension will be granted. An approved preliminary plat conditioned upon the Developer completing a "Checklist" shall also be considered to be null and void should the "Checklist" in its entirety not be completed within the designated twelve (12) month period from the date of preliminary plat approval.

e. Development Phasing

A TND Plan may be divided into phases, with the preliminary plat for each phase submitted separately. A phasing plan shall be provided when the Master Plan and Regulating Plan are submitted. The preliminary plat for the first phase shall be submitted at a date no later than one year after TND Plan approval of the Master Plan and Regulating Plan or the TND Plan shall be considered null and void. If the developer wishes to make changes to the Master or Regulating Plan after it has been adopted, the developer must resubmit the Master and Regulating Plans for approval along with the preliminary plat for the first phase or for any subsequent phases.

Any minor changes to the Master Plan or Regulating Plan may be approved by the Planning Director. Any major changes may also be approved by the Planning Director, but are subject to the review of the Planning Commission and City Council. A major change is considered to be any of the following: increasing fifty percent or more of the allocated Transect Zones to zones of higher intensity than the regulating plan; adding land parcels to the area regulated by the TND Overlay District, the Master Plan and the Regulating Plan; or a substantial deviation from the nature and character of the Master Plan. Any other changes are considered to be minor changes.

f. Replats

The developer may replat or further divide one or more lots after filing of the final plat. Any replatting shall be in accordance with the provisions of this ordinance and will not conflict with the approved TND Master and Regulating Plans. Final Plat procedures shall be followed for submission of a replat.

2.4.2 Preliminary Plat Submittal Requirements

a. General

This section denotes specific plat requirements for an entire TND or a development phase in a TND Overlay District. The Developer shall submit five (5) copies of the Preliminary Plat Plans and Preliminary Plat Data.

b. Preliminary Plat Plans

The Preliminary Plat shall be drawn clearly and legibly at a scale not smaller than 1" = 100' and shall show or be accompanied by the following information:

*Basic Data:*

- (1) Name of development and/or phase.
- (2) Name and address of owner of record, Developer and surveyor or engineer.
- (3) North arrow, graphic scale and date.

*Location:*

- (4) Vicinity Map showing location and acreage of TND or phase.
- (5) A legal description of the tract giving exact boundary lines and bearings and distances as well as the acreage to the nearest one-tenth of an acre, the date of the survey, and a preliminary survey certification.

*Topography:*

- (6) Contour intervals of not more than two (2) feet where the overall average slope is less than 4% grade and not more than five (5) feet where the slope is greater than 4%.
- (7) Natural features within the proposed TND or phase including drainage channels, bodies of water, wooded areas and other significant features. The direction of flow of all watercourses leaving the tract shall be indicated and all water courses entering the tract. The drainage area above the point of entry shall also be noted. The downstream drainage channel and drainage structures substantially impacted by this phase shall be shown.
- (8) If any portion of the land within the TND or phase is subject to flooding, the limit of such flooding shall be noted and the appropriate 100-year floodplain and/or floodway shall be identified.

*Existing Conditions:*

- (9) The present zoning classification of the land within the TND or phase and of the adjoining land contiguous to the boundary of the proposed TND or phase, and the border of the TND Overlay District.
- (10) Existing streets, buildings, watercourses, railroads, culverts, utilities, and easements on and adjacent to the tract.

*Proposed Design:*

- (11) The proposed design including streets and alleys with proposed street names, lot lines with appropriate dimensions, easements, land to be reserved or dedicated for public use, land to be used for purposes other than residential, and proposed open space.
- (12) A phasing plan including the boundaries for each phase and the location of all CAGIS monuments for the phase. A minimum of two CAGIS monuments shall be placed in each phase exceeding ten (10) acres. For phases of ten (10) acres or less in size, no new CAGIS monumentation is required. However, these smaller phases must also be tied to CAGIS monumentation

**c. Preliminary Plat Data**

Preliminary Plat Data includes additional information about the proposed development that is not covered in preliminary plat plan requirements.

*Sewer and Water:*

- (13) The source of the water supply.
- (14) Where wastewater disposal is to be accomplished by extending wastewater utility facilities, this circumstance shall be indicated on the Preliminary Plat (In those instances where extraordinary systems are proposed in lieu of extending public waste water system, detailed information shall accompany the plat.)
- (15) State Health Department approval of the water supply and/or sewage system if the requirement of the phase is to be met by any other means than by connecting to or extending the water supply or sewage system operated by the City of Conway and/or the Conway Corporation.
- (16) Improvement plans for all specified facilities and utilities.

*Bill of Assurance:*

(17) A draft of any Bill of Assurance proposed for the phase generally describing proposed covenants, restrictions and conditions applicable to the property included in the submitted plat.

*Street and Drainage Details:*

(18) Typical cross-sections of all streets and centerline profiles of approximate street grades. (Computations for engineering profiles may be required by the staff or City Engineer if deemed advisable.)

(19) Drains and drainage ways and the location, size and construction of drainage ways and structures.

(20) Typical cross section and centerline profile of all drains and drainage ways.

*Other:*

(21) Other such information as the Developer may wish to bring to the attention of the Planning Department.

*Certificates:*

(22) Each preliminary plat submitted in accord with applicable State Statutes shall include the following certificates.

## a. Certificate of Preliminary Survey Accuracy

I, \_\_\_\_\_, hereby certify that this plat correctly represents a boundary survey made by me and all monuments shown hereon actually exist and their location, size, type and material are correctly shown.

Date of Execution: \_\_\_\_\_

\_\_\_\_\_  
Registered Land Surveyor

State of Arkansas

Registration No. \_\_\_\_\_

## b. Certificate of Preliminary Plat Approval

This plat has been given preliminary plat approval only and has not been approved for recording purposes as a public record. This certificate shall expire on \_\_\_\_\_ (date).

Date of Execution: \_\_\_\_\_

\_\_\_\_\_  
Chairman, Conway Planning Commission

(23) Each set of street and drainage plans submitted in accord with applicable state statutes and in conformance with Arkansas Code Annotated 17-27-101-ET-SEQ shall include the following certificate:

## c. Certificate of Preliminary Engineering Accuracy

I, \_\_\_\_\_, hereby certify that this plan correctly represents a plan made under my direction and engineering requirements of the Conway TND Overlay District Ordinance have been complied with.

Date of Execution: \_\_\_\_\_

\_\_\_\_\_  
Registered Engineer

State of Arkansas

Registration No. \_\_\_\_\_

#### 2.4.3 Final Plat Process

When the requirements of this TND Overlay Ordinance have been satisfied and while the Preliminary Plat Approval is in effect, the Developer may submit to the Planning Director an Application for Review and Approval of the Final Plat. If the submitted final plat conforms to an approved preliminary plat and all the requirements listed in section 2.4.4 Final Plat Submittal Requirements, the plat may be reviewed, approved and signed by the Planning Director after review and approval by staff and without further review by the Planning Commission. Otherwise, the plat must be reviewed by the Planning Commission for final plat approval

##### a. Submission Requirements

The developer shall submit an application consisting of:

- (1) A letter of application requesting review and approval of the Final Plat.
- (2) The Final Plat in an original reproducible form plus eight (8) prints and other documents as specified in 2.4.4.
- (3) A filing fee of \$100.

If the final plat requires review by the Planning Commission, submission must be at least seventeen (17) days prior to the meeting at which it is to be considered.

##### b. Approval by Planning Director

The Final Plat of the proposed TND or phase shall be submitted to the Planning Director for final approval prior to expiration of the Preliminary Plat. If not submitted for final approval within such time, the preliminary plat shall be considered abandoned.

If the Developer requests permission to develop only a portion of the property for which the Preliminary Plat was approved, the Planning Director may grant approval of a Final Plat for said portion alone.

The Planning Director shall approve or disapprove the Final Plat within eighty (80) days of receipt thereof; otherwise said Final Plat shall be deemed to have been approved. Disapproval of the plat shall be transmitted to the Developer with the reasons therefore within a reasonable time (not to exceed two weeks) after the meeting at which the plat was disapproved.

The original plat and all copies shall be retained and distributed in accordance with the provisions contained in paragraphs C and D below.

Approval of the Final Plat by the Planning Director shall not constitute the acceptance by the public of the dedication of any streets or other public ways or grounds.

**c. Acceptance of Public Dedications**

Before the Final Plat is recorded in the office of the County Recorder, an agreement shall be reached between the Developer or his agent and the City Council. Said agreement shall be with regard to the installation of any street improvements or utility construction called for in the phase plat; the dedication and/or reservation of lands for public use; the dedication and acceptance of utilities and public improvements; and other agreements as required in the Planning Commission's approval of the plat. The City Council must receive one of the following prior to accepting the public dedications and before the final plat will be eligible for recording.

- (1) A certificate submitted by the Developer and approved by the City Council, stating that all required improvements and installations to the phase have been made, added, or installed; or
- (2) A cash deposit in the full amount as determined by the City, necessary to complete the improvements and installations in compliance with this Code for the portion of the phase for which final plat approval is sought. Such cash deposit may be withdrawn in direct proportion to the amount of work completed as approved by the City; or
- (3) An executed agreement with the City which shall:
  - a. Be in an amount determined by the City to be sufficient to complete the improvements and installations for the phase in compliance with this Code.
  - b. Specify the time for the completion of the improvements and installations.
  - c. Be accompanied by a performance bond or letter of credit payable to the City, which shall be satisfactory to the City Attorney as to form, sufficiency, and manner of execution. The period of time shall be specified in the resolution approving the Final Plat and shall be incorporated in the bond, and shall not in any event exceed two (2) years from the date of final approval. If improvements are not completed within the specified time, the city will collect the specified amount and complete the improvements.

**d. Recording**

Upon approval of the Final Plat and acceptance of the public dedications by the City Council, the Planning Department shall have the final plat recorded in the office of the County Recorder. The Developer shall submit the original or a mylar reproducible plus twelve (12) copies to the Planning Department and pay all fees in connection with the recording of said plat.

The Final Plat shall be filed in the office of the County Recorder within two (2) years after approval by the Planning Commission; and if not filed within such time, said approval shall be considered as having been abandoned.

Upon recording the plat, the designated City official shall retain the original tracing and one (1) copy for the Planning Commission's files, one (1) copy shall be forwarded to the Tax Assessor, and one (1) copy shall be returned to the Developer.

**2.4.4 Final Plat Submittal Requirements**

The Final Plat shall be drawn in black ink on mylar film, tracing paper, or other comparable transparent material and on sheets whose dimensions are either 18" x 24" or 24" x 36". The scale shall be either 1"



= 100' or 1"=50'. The original tracing or reproducible mylar and eight (8) prints shall be submitted to the Planning Director. The drawing shall be neat, legible, and suitable for filing for record in the office of the Circuit Clerk-Recorder. Patching and pasting of paper or other attachments to the plat is not acceptable. Allowance shall be made for a 1/2" border at the top, bottom, and right edges of the sheets and a 1 1/2" border at the left edge of the tracing pages. When more than one sheet is used for a plat, a key map showing the entire phase on a smaller scale shall be denoted on the first sheet. The final plat must also be submitted to the Planning Department in a computer compatible media and in a format as determined by the Planning Department. Payment for any required street signs must be made to the Street Department prior to the filing of the plat.

The Final Plat shall show or be accompanied by the following information:

- (1) Name of the owner and developer.
- (2) Name of the Registered Land Surveyor making the survey and preparing the plat.
- (3) Name of the TND or phase and adjacent phases.
- (4) The names of all streets.
- (5) The identifying numbers of lots and blocks in accord with a systematic numbering system.
- (6) North arrow, date, scale, and acreage being subdivided.
- (7) A certified boundary survey of the property with bearings and distances referenced to survey lines and established phases with complete and accurate field notes of said boundaries. The lines with dimensions of all adjacent land, streets, alleys, easements and adjacent phases shall be shown. Adjusted Arkansas State Plane Coordinates (NAD-83 (1997)) shall be shown for all boundary corners and all corners of record utilized, along with a statement indicating the ratio error of closure of the fieldwork. All CAGIS monuments shown on the approved preliminary plat for the phase being final platted shall be monumented with approved monuments.
- (8) Location of lots, streets, alleys, sidewalks, easements, building setback lines (both front and side streets) and other features shall be shown with dimensions. The purpose of the easements shall be shown on the plat, i.e., all easements are for utilities and storm drainage. A note(s) shall also be shown indicating the responsibility of sidewalk construction (Developer and/or Homeowner/Builder).
- (9) All necessary dimensions including linear, angular, and curvilinear dimensions shall be shown in feet and decimals of a foot. The angular dimensions shall be shown by true bearings and degrees, minutes, and seconds. The length of all-straight lines, deflection angles, radii, tangents, central angles or curves and cords and arcs of curves shall be shown. All curve information shall be shown from the centerline of the street based on arc dimensions. Dimensions shall be shown from all angle points and points of curve of lot lines. All lots on curves shall be shown with curve length dimensions based on arc dimensions.
- (10) The location of all survey monuments shall be indicated on the plat and the true courses and distance to the two (2) nearest established section corners or benchmarks or other recognized permanent monuments shall accurately be denoted.
- (11) Certification that all fees have been paid.
- (12) Construction drawings of all street improvements, sidewalks, storm drainage facilities, and public improvements shall accompany the final plat. As-built drawings of these improvements shall be submitted to the Street Department no less than one year after improvements are installed.
- (13) Copy of any restrictive covenants and the Bill of Assurance for the property. Such document(s)

shall incorporate the same provisions as those filed with the Preliminary Plat including but not necessarily limited to the following: offering dedication of streets and alleyways, parks and other public lands, establishing easements, setting forth privileges and conditions pertaining thereto, and setting forth the restrictions and covenants of the phase setting forth procedures by which amendments to the Bill of Assurance can be made, and said Bill of Assurance shall contain reference to the approval of the Final Plat.

(14) Letters shall be submitted by the Conway Corporation and by each utility company which will have service lines in the proposed phase stating the utilities have been installed or assurances given by utility company that improvements will be installed to the satisfaction of the City.

(15) Each Final Plat submitted to the Planning Director shall be in accord with applicable State Statutes and carry the following certifications printed thereon:

a. Certificate of Owner

We the undersigned, owners of the real estate shown and described herein, do hereby certify that we have laid off, platted, and subdivided and do hereby lay off, plat, subdivide said real estate in accordance with this plat and do hereby dedicate to the use of the public the (streets, alleys, drives, easements, etc.) as shown on said plat."

Date of Execution: \_\_\_\_\_

Signed : \_\_\_\_\_

Name & Address

Source of Title: D.R. \_\_\_\_\_

Page \_\_\_\_\_

b. Certificate of Recording

This document filed for record \_\_\_\_\_ day, 200\_\_\_\_ in Plat Book No. \_\_\_\_\_, page \_\_\_\_\_.

Signed \_\_\_\_\_

Circuit Clerk

c. Certificate of Surveying Accuracy

I, \_\_\_\_\_, hereby certify that this plat correctly represents a boundary survey made by me and boundary markers and lot corners shown hereon actually exist and their location, type and material are correctly shown and all minimum requirements of the Arkansas Minimum Standards for Land Surveyors have been met.

Date of Execution: \_\_\_\_\_

Signed \_\_\_\_\_

Registered Land Surveyor

No. \_\_\_\_\_

State of Arkansas

d. Certificate of Final Plat Approval

Pursuant to the Conway TND Overlay District Ordinance and all other conditions and approval having been completed, this document is hereby accepted. This Certificate is hereby executed under the authority of the said rules and regulations.

Date of Execution: \_\_\_\_\_

Signed \_\_\_\_\_

Conway Planning Director

#### **2.4.5 SURVEY STANDARDS**

This TND Overlay District Ordinance hereby references the Conway Subdivision Ordinance, ARTICLE III MINIMUM SURVEY STANDARDS, to be the required survey standards for this Overlay District.

### **2.5 PROCEDURE FOR BUILDING PLAN REVIEW**

2.5.1 Prior to building plan review, the Architecture and Landscape Patternbook must be reviewed by the planning staff and approved by the Planning Director. The City of Conway shall not issue a building permit until the Architecture and Landscape Pattern book is approved.

2.5.2 The Architecture and Landscape Patternbook will be a revised and updated version of the Preliminary Architecture and Landscape Patternbook required for TND Plan Approval. This patternbook will include building style, shape and design requirements and recommendations to which all buildings within the TND Overlay District must adhere. Any changes to this patternbook after approval may be made by Minor Exception.

2.5.3 All building plans must be approved by the Design Review Board established by the Developer. The Developer and the Design Review Board shall approve all building plans prior to construction. The City of Conway shall issue a Building Permit only upon presentation of proof of preliminary architectural and landscape approval by the Design Review Board. The Design Review Board shall approve the buildings for compliance to the Architecture and Landscape Patternbook standards for the TND Overlay District. The City of Conway shall issue a Certificate of Occupancy only upon presentation of proof of final architectural approval by the Design Review Board. See Sections 4.3 and 4.4 for more information about the architectural and landscape standards.

### **2.6 MINOR EXCEPTIONS AND MAJOR EXCEPTIONS**

2.6.1 There shall be two levels of deviation from the requirements of this TND Overlay District Ordinance: Minor Exceptions and Major Exceptions.

2.6.2 Minor Exceptions are minor departures from the TND Overlay District standards or the approved TND Plan and shall be granted administratively through the Conway Planning Department. A Minor Exception is a ruling that would permit a practice that is not consistent with a specific provision of this TND Overlay or the approved TND Plan but is justified by the Intent (Section 1.2).

2.6.3 The Director of Planning shall review the request for a Major Exception with input from the DRC (Design Review Committee) as needed to determine if the Major Exception should be granted. If the Planning Director decides to grant the request, he/she must, on the same day, notify all the City Council members

## TND OVERLAY

### City of Conway, Arkansas

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of that determination by letter, email, telephone contact, or by placement of a notice in their mail boxes at City Hall or in another manner approved by the Mayor. If any one of the City Council members feels that the Major Exception should not have been granted, the Council member must notify the Planning Director within no less than five (5) working days from the date of the Planning Director's decision to grant the request. Upon such notification, the Planning Director shall refer the request to the Planning Commission for review at the next scheduled meeting of the Planning Commission. If the Planning Commission should also decide to grant Major Exception, the request shall be considered granted.

If the Planning Director decides to refuse the Major Exception, the applicant may appeal to the Planning Commission no less than thirty (30) working days from the date of the Planning Director's decision to refuse the Major Exception. The Planning Director must notify the applicant of the decision to refuse the request by mail, on the same day of the decision. The appeal shall be placed on the agenda of the next scheduled meeting of the Planning Commission.

If the Planning Commission refuses the Major Exception, the applicant may appeal the decision to the City Council. The appeal must be submitted to the Planning Department no less than thirty (30) working days from the date of the Planning Commission's decision. The appeal shall be placed on the agenda of the next scheduled meeting of the City Council.

The Planning Director or Planning Commission or City Council will grant the Major Exception only when the requirements noted above are suitably demonstrated. The Major Exception will be in harmony with the general purpose and intent of the TND and will not be injurious to the neighborhood or otherwise detrimental to the public welfare.

- 2.6.4 The request for a Major Exception shall not subject the entire application to public hearing, but only that portion necessary to rule on the issue under consideration if the Planning Commission and/or City Council decide it is necessary to review the decision of the Planning Director.
- 2.6.5 Minor Exceptions and Major Exceptions shall be considered unique and shall not set precedent for others.

**ARTICLE 3. TRADITIONAL NEIGHBORHOOD DEVELOPMENT PLANS****3.1 INSTRUCTIONS**

- 3.1.1 Traditional Neighborhood Development Plans shall:
- be structured as one or several Pedestrian Sheds (1/4 mile radius).
  - allocate the Transect Zones and Densities.
  - lay out the Thoroughfare network according to the provisions of Section 3.6 and Table 3.
  - include a set of building standards prepared in accordance with the requirements of Article 4.
- 3.1.2 Traditional Neighborhood Development Plans may be prepared by an owner, a developer, or by the City of Conway/Conway Planning Department.

**3.2 TRANSECT ZONES**

- 3.2.1 Transect Zones shall be constituted of the elements described in Table 1 and the standards summarized in Table 10. Transect Zones shall be allocated in a TND Plan, consisting of a Master Plan and Regulating Plan. Transect Zones shall be shown on the Regulating Plan. After a TND Plan is adopted, the Transect Zones specified in this TND Ordinance shall constitute the official zoning standards according to the Regulating Plan.

**3.3 DENSITY CALCULATIONS**

- 3.3.1 The density allowances in this TND Overlay District shall supersede any density specifications in the Conway Zoning Ordinance or Subdivision Ordinance.
- 3.3.2 The Density for Residential shall be limited by Transect Zone according to Table 10. Density shall be calculated as gross density of dwelling units within each Transect Zone. For purposes of Density calculation, the Transect Zone Areas include the Thoroughfares but not land allocated to Civic Function. The provision of habitable space in an Outbuilding shall not be considered a second dwelling unit.
- 3.3.3 The Density for Lodging, Office and Retail shall be constrained by the combination of maximum lot coverage, building setbacks and maximum height in Table 4 and Table 10 and the required parking in Table 8.
- 3.3.4 Greater density shall be granted by Major Exception.

**3.4 ENVIRONMENTAL REQUIREMENTS**

- 3.4.1 General
- Transect Zones manifest a range of natural and urban conditions. In case of conflict, to the extent not inconsistent with applicable state or federal law, the natural environment shall have priority in the more rural zones (T1-T2) and the built environment shall have priority in the more urban zones (T3-T6).
  - The modification of the natural conditions shall comply with all state and federal regulations and guidelines.



**3.4.2 Impermeable Surface**

Impermeable surface is limited by a maximum ratio of lot coverage. Lot coverage by buildings, parking or other impervious surfaces shall not exceed the maximum ratio allowed in Section 4 and in Table 10.

**3.4.3 Stormwater Management**

a. T3: To the extent not inconsistent with applicable state or federal law, the management of storm water may be primarily through retention, detention and percolation on the individual lot, through swales in the Public Frontage, or primarily off-site through underground storm drainage, or through other best-management practices.

b. T4-T6: To the extent not inconsistent with applicable state or federal law, management of storm water shall be primarily off-site through underground storm drainage or other best management practices, and there shall be no retention or detention required on the individual lot. Where necessary to meet state or federal law, off-site stormwater management or mitigation measures may be implemented.

**3.5 UTILITIES**

3.5.1 Utilities should be constructed by the Developer as follows: In general, the pipe (wet) utilities should run along the frontage of the lots, and the conduit (dry) utilities should run along the rear of the lots, whenever an alley or lane is provided. The utilities may be laid in easements and beneath the paving.

a. The wet utilities of sanitary sewer, storm drainage, water supply, and gas lines should be located within the thoroughfare rights-of-way (along the lot frontages).

b. If alleys or lane easements exist, the dry utilities of power, cable, and telephone conduits should be located within the alley or lane easements. If an alley or lane easement does not exist, the dry utilities should be located at the rear of the lots or within rear parking areas. The utility pedestals may be located within the right-of-way or within the rear setback of the lots.

**3.6 STREETSCAPE REQUIREMENTS****3.6.1 General**

a. The Streetscape is the urban element that establishes the public realm. The Streetscape consists of Thoroughfares, Public Frontages, and Private Frontages. Thoroughfares are considered to be travel lanes for vehicles and bicycles; parking lanes for cars; and sidewalks or paths for pedestrians. Thoroughfares are intended for use by vehicular and pedestrian traffic. Thoroughfare standards are detailed in Section 3.6.2 and Tables 2A, 2B, and 2C. Public Frontages include the area between the curb and the Frontage Line that includes but is not limited to the sidewalk, planters, street trees and street lights. Public Frontages are detailed in Section 3.6.4 and Tables 3A and 3B. The Private Frontages include the building facades, yards, porches, fences, awnings or other conditions that impact the public realm. Private Frontages are explained in Section 3.6.4 and Table 5.

b. Within the more rural Transect Zones (T1 through T2) pedestrian comfort shall be a secondary consideration of the Thoroughfare. Design conflict between vehicular and pedestrian movement shall be generally decided in favor of vehicular mobility.

c. Within the more urban Transect Zones (T3 through T6) pedestrian comfort shall be a primary consideration of the Thoroughfare. Design conflict between vehicular and pedestrian movement shall be decided in favor of the pedestrian.

## 3.6.2 Thoroughfares

a. The standards for vehicular lanes shall be guided by Tables 2A, 2B and 2C. Table 2A provides a range of lane widths that may be used to modify the sample vehicular lane assemblies in Table 2B that together compose the thoroughfare assemblies in Table 2C.

b. The street width shall be measured from edge of curb to edge of curb or edge of pavement to edge of pavement where curb does not exist. All parking lane widths include the gutter pan.

c. The following thoroughfare types are recommended for the construction of new neighborhoods and should be designed in a manner that responds to the adjacent uses.

1. Boulevard (BV): a thoroughfare designed for high vehicular capacity and moderate speed that is typically located in T5 and T6. Boulevards are long-distance thoroughfares that serve a city-wide function by connecting urbanized areas. Boulevards typically have at least four travel lanes (two in each direction) and may have parallel parking. Boulevards are usually equipped with slip roads that buffer the sidewalks and buildings or alternately may have a median. The Public Frontage has curb, gutter, sidewalk and trees located in either planters or tree wells. Boulevards become arterials upon exiting urban areas.

2. Avenue (AV): a thoroughfare of high vehicular capacity and low speed. Avenues are short distance connectors between urban centers. Avenues typically have at least two travel lanes (one in each direction) and parallel parking. Avenues may be equipped with a landscaped median. The Public Frontage has curb, gutter and sidewalk. When located in T3 and T4, Avenues usually have trees located in planters and when located in T5 and T6, Avenues usually have tree wells and wider sidewalks. Avenues become collectors upon exiting urban areas.

3. Urban Street (US): a thoroughfare of low speed and capacity that is located in T5 and T6. Urban Streets are local thoroughfares in urban centers with more intensive commercial or civic uses. Urban Streets may have one or two-way travel, typically with one or two travel lanes and parallel or angle parking. The Public Frontage has curb, gutter and wider sidewalks. Urban Streets may have trees located in tree wells.

4. Street (ST): a local urban thoroughfare of low speed and capacity that is located in T3, T4 and T5. Streets are local thoroughfares in urban areas with predominately residential uses. Streets may have one or two-way travel, typically with one or two travel lanes and parallel parking. The Public Frontage has curb, gutter, sidewalks and trees located in planters.

5. Road (RD): a thoroughfare of low vehicular speed and capacity that is located in T3 and T4. Roads are local and suburban thoroughfares with low intensity residential uses. Roads usually have two-way travel and do not usually have parking. The Public Frontage may have curb and gutter or swales; sidewalks, walking paths or bicycle trails; and trees located in planters or natural clusters.

d. Rear access to buildings should also be provided, with the design of the access lane to be based on its context. The following standards should be used to provide different types of access.

1. Rear Alley (AL): a vehicular driveway located to the rear of lots providing access to service areas and parking, and containing utility easements. Rear Alleys are intended for more intensive uses (typically T5 and T6 zones). Rear Alleys should be paved for the full right-of-way width, with drainage typically by inverted crown.

2. Rear Lane (LA): a vehicular driveway located to the rear of lots providing access to parking and out-buildings and containing utility easements. Rear Lanes are intended for less intensive uses (typically T3 and T4 zones). Rear lanes may be paved lightly to driveway standards. Its streetscape consists of

gravel or landscaped edges, no raised curb and is drained by inverted crown or percolation.

- e. Bicycle travel may also be included in the thoroughfare design, based on the following standards.

1. Bicycle Lane (BL): a dedicated bicycle lane running within a moderate-speed vehicular thoroughfare, demarcated by striping.

2. Bicycle Route (BR): a thoroughfare suitable for the shared use of bicycles and automobiles moving at low speeds.

3. Bicycle Trail (BT): a bicycle way running independently of a high-speed vehicular thoroughfare.

- f. Pedestrian only travel may also be included in the thoroughfare design, based on the following standards.

1. Passage (PS): a pedestrian connector passing between buildings, providing shortcuts through long blocks and connecting rear parking areas to frontages. Passages may be roofed over.

2. Path (PT): a pedestrian way traversing a park or rural area, with landscape matching the contiguous open space. Paths should connect directly with the urban sidewalk network.

### 3.6.3 Thoroughfares and Blocks

- a. All Thoroughfares should terminate at other Thoroughfares, forming a network. Internal Thoroughfares shall connect wherever possible to those on adjacent sites. Cul-de-sacs shall be permitted only by Minor Exception when necessary to accommodate natural site conditions.

b. The Thoroughfare network shall be designed to define blocks not exceeding the size prescribed in Table 10. The size shall be measured as the sum of lot Frontage Lines. These Frontage Lines may include those that front on a vehicular Thoroughfare, such as a Street or an Avenue, or a pedestrian Thoroughfare, such as a Pedestrian Path.

- c. Block size may be adjusted by Minor Exception.

### 3.6.4 Public Frontages

- a. The Public Frontage is the area between the curb of a vehicular lane and the Frontage Line. Elements of the Public Frontage include the curbs, sidewalks, planters, street trees and streetlights. Public Frontages should be based on the types shown in Tables 3A and 3B and allocated within Transect Zones as specified in Table 10C. See Section 3.6.2 for more explanations of Thoroughfares and their respective Public Frontages.

b. Public Frontages may be adjusted by Minor Exception if the site conditions or unique design circumstances necessitate a change from the types shown in Tables 3A and 3B.

c. Elements of the public frontage are encouraged to use attractive or creative design elements such as brick or colored pavers in sidewalk design; pedestrian-scaled and decorative street lights; and street furniture.

### 3.6.5 Private Frontages

- a. The Private Frontages that define the portion of a building facing a Thoroughfare shall generally follow Table 5 and Table 10I. The variables of Private Frontage are the depth of the setback and the combination of architectural elements such as fences, stoops, porches, entrances, windows, awnings and galleries.

b. Private Frontages may be adjusted by Minor Exception if the site conditions or unique design circumstances necessitate a change from the types shown in Table 5.

**3.7 CIVIC FUNCTIONS****3.7.1 General**

- a. Places for public use shall be required for each community and designated on the TND Plans as Civic Space(s) and/or Civic Building(s).
- b. Civic Spaces are public sites permanently dedicated to usable open space.
- c. Civic Buildings are sites dedicated for buildings generally operated by not-for-profit organizations dedicated to culture, education, government, transit and municipal parking, or for a use approved by Minor Exception.

**3.7.2 Civic Space Specific to T3-T6 Zones**

- a. Civic Spaces shall be designed as generally described in Table 9 and allocated to zones as described in Table 10d.
- b. Each Civic Space shall have public access and should have a minimum of 25% of its perimeter enfronting a Thoroughfare.

**3.7.3 Civic Buildings Specific to T3-T6 Zones**

- a. A Civic Building serves as a landmark and a public gathering place. These buildings should be constructed as permanent additions to the long-term vibrancy of the Town and should serve to exemplify the very best architectural designs and building practices.
- b. Civic Building sites should be located at prominent places, such as within or adjacent to Civic Spaces, or at the axial termination of significant Thoroughfares. Lots for future Civic Buildings are encouraged to be reserved for later construction if appropriate.

## ARTICLE 4. BUILDING SCALE PLANS

### 4.1 INSTRUCTIONS

4.1.1 Lots and buildings located within this TND Plan shall be subject to the requirements of this Article.

4.1.2 The requirements described in this Article shall control the configuration, density, setbacks, height and function of buildings, as well as their architectural, landscape, parking, and signage standards.

### 4.2 BUILDING CONFIGURATION

#### 4.2.1 General

a. Buildings shall be located in relation to the boundaries of their lots according to Table 4.

b. All lots shall front on public streets or public space. Facades should be built parallel to a Principal Frontage Line or parallel to the tangent of a curved Principal Frontage Line. Lots shall enfront a vehicular Thoroughfare, except 20% of the lots within each Transect Zone may enfront a pedestrian passage.

#### 4.2.2 Lot Requirements

a. Lots shall be dimensioned by Transect Zone according to Table 4. The lot width may vary by 5% if allowed by Minor Exception.

b. Lot coverage by buildings, parking or other impervious surfaces shall not exceed the maximum ratio allowed in Table 10.

c. Lots are not restricted to the number of Principal Buildings or Outbuildings per lot nor are lots required to be contiguous; however the configuration of all buildings should follow the principle and intent of Table 4 in order to form a similar plan consistent with the adjacent sites in the same Transect Zone.

d. In Table 4, Outbuildings are not shown in T6 and are only permitted by Minor Exception.

e. Lot coverage may only be exceeded if approved by Minor Exception.

#### 4.2.3 Building Density

a. The Density for Residential shall be limited by Transect Zone according to Table 10.

b. The Density for Lodging, Office and Retail shall be constrained by the combination of maximum lot coverage, building setbacks and maximum height in Table 5 and the required parking in Table 8.

#### 4.2.4 Building Setbacks

a. Setbacks for Principal Buildings shall be as shown in Table 4 and Table 10. In the case of an Infill lot, Setbacks may match one or the other of the existing adjacent Setbacks. Setbacks may otherwise be adjusted by Minor Exception.

b. Arcades and Awnings may encroach the public sidewalk without limit. Arcades, if provided, should be located as close to the back of curb as possible. However, Arcades should also be designed to avoid the swing of car doors parked parallel to the arcade and meet ADA clearance requirements.

c. Buildings with Arcades on public right of way may build an additional two (2) stories on top of the Arcade.

d. Stoops may encroach 100% of the depth of a Setback. Open porches and awnings may encroach up to



50% of the depth of the Setback. Balconies and bay windows may encroach up to 25% of the depth of the Setback.

e. Handicapped Ramps may encroach 100% of the depth of the front, side or rear Setbacks.

f. Civic buildings may be exempt from the front yard setback requirements by Minor Exception.

g. Rear Setbacks for Outbuildings shall be a minimum of 6 feet from the rear lot line if the Outbuilding and/or driveway parking is intended to be perpendicular to the Alley or Rear Lane. Rear Setbacks for Outbuildings shall be a minimum of 2 feet from the rear lot line if the Outbuilding and/or driveway parking is intended to be parallel to the Alley or Rear Lane. In the absence of Rear Alley or Lane, the rear Setback shall be as shown in Table 4.

h. Specific to T5 and T6: Buildings shall have their principal pedestrian entrances near the Frontage Line.

i. The Principal Façade of a building should generally follow the Private Frontage types specified in Section 3.6.5 and Table 5.

#### 4.2.5 Building Heights

a. Building Heights shall be regulated by Table 4 according to Transect Zone.

b. Building Height is measured as the vertical distance above the highest sidewalk elevation measured to the base of a parapet or roof line of a flat roof, the eave of a pitched roof or the deck line of a mansard roof. Where building heights are measured in stories, no story height shall exceed 15 feet.

c. All specified Building Heights may be increased by the base elevations required by applicable FEMA standards.

d. The height limitations shall not apply to unoccupied areas of civic buildings, such as a dome, spire or similar architectural element, or to utility towers

e. Specific to T5 and T6: A first level Residential Function shall be raised a minimum of 2 feet from the average sidewalk grade.

f. Building height may be increased by 50% by Minor Exception. Any building height increase over 50% may be increased only by Major Exception.

g. All buildings over 6 stories are subject to the following conditions. The first 6 stories, constituting the building base, must follow the setback requirements in Table 4. All stories above the first 6 stories must be recessed 20 feet from the front and side street Facades.

#### 4.2.6 Building Function

a. Building Functions shall be regulated by Table 10 according to Transect Zone. Uses may be either permitted by Right, by Minor Exception or not permitted.

b. To obtain a Minor Exception, an applicant shall follow the procedure in Section 2.6.

#### 4.2.7 Screening

a. Any commercial outdoor storage shall be screened from the view from any Frontage by a Streetscreen. A Streetscreen shall be considered to be a freestanding wall built along the frontage line, or coplanar with the façade that masks storage from the thoroughfare. Streetscreens should be between 3.5 and 8 feet in height. They should be constructed of a material matching the adjacent building façade or may be a hedge or fence. Streetscreens should have openings no larger than is necessary to allow automobile and pedestrian access. In addition, all streetscreens over 4 feet high should be permeable or articulated to avoid blank walls.

b. Containment areas for trash and recyclables, including but not limited to compactors, dumpsters, com-

mercial roll-out bins, and areas for storing cardboard, shall be located and designed so as not to be visible from adjacent streets and properties and shall be placed in side or rear yards only. Containment areas on corner lots shall be located and designed so as not to be visible from the principle street and the secondary street.

#### **4.3 ARCHITECTURAL STANDARDS**

a. Architectural Standards will be provided in a Patternbook with the preliminary plat application. The standards must be approved by the planning director. All buildings shall follow the requirements of the Architectural Standards.

b. The Developer is responsible for ensuring compliance with all Architectural Standards and will appoint a Design Review Board to help enforce the standards. The Developer and the Design Review Board shall approve all building plans prior to construction. The City of Conway shall issue a building permit only upon presentation of proof of preliminary architectural approval by the Design Review Board. The Design Review Board shall approve the buildings for compliance to these standards. The City of Conway shall issue a Certificate of Occupancy only upon presentation of proof of final architectural approval by the Design Review Board. After project completion, the Developer is responsible for establishing a permanent Design Review Board or similar establishment to review substantial changes or modifications to buildings in the future.

#### **4.4 LANDSCAPE STANDARDS**

a. T3-T4: A minimum of one tree should be planted within the First Layer of every lot for each 40 feet of Frontage Line where the building type permits. (See First Layer illustration in Table 11.)

b. T5-T6: The First Layer (Table 11) should be landscaped and/or paved to match the Public Frontage. See Tables 3A, 3B and 5 for illustrations.

c. Landscape Standards will be provided in a Patternbook with the preliminary plat application. The standards must be approved by the planning director. The landscape plans shall follow the requirements of the Landscape Standards.

d. The Developer is responsible for ensuring compliance with all Landscape Standards. The Design Review Board appointed by the Developer to ensure compliance with the Architectural Standards may also review the landscape plans. The Developer and the Design Review Board shall approve all landscape plans prior to construction. If the Developer or Design Review Board find that the landscape installation does not adhere to the landscape plan or is insufficient, they may require a one-year punchlist that the landscape contractor must complete.

#### **4.5 PARKING STANDARDS**

##### **4.5.1 Parking Ratios**

Vehicular parking shall be required according to Table 8, as specified by use and Transect Zone.

##### **4.5.2 Variations in Parking Requirements**

a. On-street parking. On-street parking available along the Frontage Lines that correspond to each lot shall be counted toward the parking requirement of the building on the lot.

b. Shared parking. Shared parking may be provided according to the reduction for uses that have different peak parking requirements. Parking requirements may be reduced according to the Sharing Factor shown in Table 8. Shared parking is subject to the following standards:

1. The peak usage of the parking facility by one use is at a different time than the peak usage by another use.

2. The joint use of shared off-street parking between two or more uses may be made by contract between two or more adjacent property owners.
- c. Satellite parking
  1. The required parking may be provided within one-quarter mile of the site that it serves. Required parking may be provided at distances greater than one-quarter mile if approved by Minor Exception.
  2. A parking lot used only for occasional use (use that occurs two or fewer days per week) or is temporary in nature (not exceeding 24 months), may be exempted from providing off-street parking by Minor Exception if sufficient on-street parking can be shown to exist within one-quarter mile.
  3. A safe, direct, attractive, lighted and convenient pedestrian route shall exist or be provided between the off-site parking and the use being served.
- d. Alley Parking. Parking in an alley is not prohibited; however space used for parking in an alley may not count toward parking requirements and is intended as overflow parking.
- e. Change in Use. Developments which do not involve an increase of more than 25 percent in the building floor area or which will not increase the number of parking spaces required under this chapter by more than 25 percent may be exempted from any parking standard increase by Minor Exception.

#### 4.5.3 Loading Space

- a. Off-street loading space shall be provided for all retail or other commercial uses that receive regular delivery or shipping of goods, merchandise or equipment to the site by trucks. Loading space may not be used as parking.
- b. Loading docks and service areas shall be permitted on Frontages only by Minor Exception.
- c. Live-work units may use any alley or driveway access for loading space and are not required to have a separate loading space.

#### 4.5.4 Parking Design Standards

- a. Disabled parking shall be provided in accordance with the Americans with Disabilities Act.
- b. Adjacent parking lots should be interconnected except in the case of existing steep topography between sites. Each parking area that is interconnected may reduce its minimum parking requirement by five percent.
- c. Standard off-street parking spaces should be 9 feet by 18 feet but may be adjusted as needed.
- d. Parking should be accessed by an Alley or Rear Lane.
- e. Parking and loading areas shall be masked from the Frontage by a Liner Building or Streetscreen.
- f. Location
  1. T3: Open parking areas shall be located at the Second and Third Lot Layers, as shown in Tables 4 and 11. Driveway aprons and drop-offs may be located at the First Layer. Outbuildings that include garage space shall be located in the Third Layer.
  2. T4: All parking areas except for Driveways shall be located in the Third Layer as illustrated in Tables 4 and 11. Parking areas should generally be accessed via rear lane. Driveway entrances are allowed within the first layer if a rear lane or alley is not provided; however driveway entrances should be limited to 15 feet or less. Outbuildings that include garage space shall be located in the Third Layer.
  3. T5-T6: All parking areas shall be located in the Third Lot Layer as illustrated in Tables 4 and 11. Parking areas should generally be accessed via rear alley or lane. The vehicular entrance of a parking lot or

garage on a Frontage shall be no wider than 30 feet. Pedestrian entrances to all parking lots and parking structures open to the public shall be directly from a Frontage Line. Where applicable, Outbuildings that include garage space shall be located in the Third Layer.

g. Off-street parking lots shall include trees, plants and other landscaping provisions. This landscaping shall be specified in greater detail in the Landscape Patternbook.

#### 4.5.5 Parking Construction Standards

a. Parking and driving area surfaces may include asphalt, concrete, unit pavers or other dust-free paving treatment.

b. Porous or permeable paving materials are also permitted for parking and driving area surfaces. Permeable pavement areas may be exempted from the maximum lot coverage requirement by Minor Exception.

c. Gravel shall be permitted by Minor Exception.

d. Curbing is recommended for all parking areas. It may be rolled curb or stone curb. Other types may be approved by Minor Exception.

### 4.6 SIGNAGE STANDARDS

#### 4.6.1 General

a. These signage standards shall regulate the design, height, location and size of signs to be visually complementary and compatible with the scale, and architectural style of the primary structures on the site. These standards shall supersede the Conway Sign Ordinance; however the Conway Sign Ordinance shall be referred to for provisions not covered in this section. The process for sign approval shall be as stated in the Conway Sign Ordinance, Section 6.

b. One address number no more than 6 inches measured vertically shall be attached to the building in proximity to the principal entrance or at a mailbox.

c. Any other types of signage not specified below may only be approved by Minor Exception and must be visually complementary and compatible with the character of the other signage.

d. Billboards are prohibited.

#### 4.6.2 T3-T4 Standards

a. One blade sign for each business may be permanently installed perpendicular to the Façade and shall not exceed a total of 4 square feet. Blade signs may be attached to the building or suspended from a post.

b. Signage may be externally lit in T4. Signage shall not be lit in T3 unless permitted by Minor Exception.

#### 4.6.3 T5-T6 Standards

a. Blade signs, not to exceed 8 square feet for each separate business entrance, may be attached perpendicular to the Façade.

b. One Wall Sign may be placed on each Façade. The Wall Sign should be sized not to exceed 5% of the aggregate area of each Façade or 3 feet in height by 95% of the building Façade length, whichever is greater. For buildings with multiple tenants or businesses, the total wall signage permitted per façade may be divided amongst all building tenants.

c. An Awning Sign may cover up to 50% of any total awning area. Awnings are permitted on any storefront as shown in Table 5.

- d. Window Signs located within a building are limited to 25% of the total window area.
- e. A-frame Signs ("sandwich board" signs) less than three feet in height and two feet in width are allowed on a sidewalk immediately in front of a business.
- f. Signage shall be externally lit, except that Window Signs may be neon lit.

#### 4.6.4 Civic Uses

- a. Civic Uses may have one Monument Sign that is less than 24 square feet per side and less than 4 feet in height.
- b. Civic Uses may also have one Wall Sign on each Façade. The Wall Sign should be sized not to exceed 5% of the aggregate area of each building Façade.
- c. Signage shall be externally lit.

#### 4.7 SPECIAL REQUIREMENTS

- 4.7.1 A TND Plan may assign a special requirement that buildings along active streets may be designated for Retail Frontage with a Shopfront at sidewalk level along the entire length of the Frontage. The Shopfront shall be no less than 50% glazed in clear glass and provided with an awning or an arcade overlapping the sidewalk as generally illustrated in Table 5. The first floor shall be confined to Retail use through the depth of the First Layer.



**ARTICLE 5. INAPPLICABILITY OF CERTAIN REGULATIONS****5.1 GENERAL**

- 5.1.1 Within this TND Overlay District Ordinance, certain Regulations set forth in the City of Conway Zoning Ordinance and City of Conway Subdivision Ordinance shall either not apply or shall be modified, as follows. Where the City of Conway Zoning Ordinance and/or City of Conway Subdivision Ordinance are in conflict with the intent of this TND Overlay District, (Section 1.2), the TND Overlay District standards shall prevail.

**5.2 INAPPLICABILITY OF SPECIFIED SUBDIVISION REGULATIONS**

- 5.2.1 Curb radius shall be not less than five (5) feet instead of thirty-one and one-half (31 ½) and twenty-eight (28) feet curb radii for connections with collector, arterial or residential streets as required in Subdivision Ordinance Article IV Section 5-A(10)a.
- 5.2.2 Property line corners may be square instead of rounded as set forth in Subdivision Ordinance Article IV Section 5-A(10)d.
- 5.2.3 The minimum rights-of-way, and minimum street widths shall conform to Section 3.6 and Table 2 of this TND Overlay District instead of the values in Subdivision Ordinance Article IV Section 5-A(11) Table 2.
- 5.2.4 The paved width of alleys (lanes) may be not less than eight (8) feet rather than not less than sixteen (16) feet as set forth in Subdivision Ordinance Article IV Section 5-B(2).
- 5.2.5 Parking is allowed in alleys unless the Property Owners Association determines that no parking shall be allowed instead of the no parking requirement of Subdivision Ordinance Article IV Section 5-B(4).
- 5.2.6 Easements may be less than ten (10) feet in width if reviewed by the City Engineer and/or the Conway Corporation and approved by the Planning Director instead of the ten (10) foot minimum as set forth in Subdivision Ordinance Article IV Section 5-C(1).
- 5.2.7 Blocks of less than two hundred (200) feet in length shall be prohibited, instead of the four hundred (400) foot minimum set forth in Subdivision Ordinance Article IV Section 5-E(2).
- 5.2.8 The setback requirements in Article 4 and Tables 4 and 10 of this TND Overlay District, shall apply instead of the setbacks set forth in Subdivision Ordinance Article IV Section 5-F(4).
- 5.2.9 Curbs and gutters may have a twelve (12) inch pan with a rectangular or trapezoidal curb instead the twenty-four (24) inch wide gutters with four (4) inch high roll curb as set forth in Subdivision Ordinance Article V Section 2-G.
- 5.2.10 Open ditches on the site may be restored to natural stream or landscaped stream conditions instead requiring concrete bottom and concrete sidewalls, or equal, as set forth in Subdivision Ordinance Article V Section 4-(5).
- 5.2.11 Detention basins on the site may be restored to natural stream or landscaped stream conditions instead requiring concrete bottom and concrete sidewalls, or equal, as set forth in Subdivision Ordinance Article V Section 4-(12).
- 5.2.12 There is no minimum or maximum distance the sidewalk edge must be from the curb, instead of the minimum of five and one-half feet from the back of curb as set forth in Subdivision Ordinance Article V Section 9-(13)(C).
- 5.2.13 Street lighting designed to minimize light pollution is recommended, generally involving lower intensities and shielding or cut-off. In case of conflicts with current standards and spacing prescribed by the Conway Corporation, the developer is encouraged to negotiate revisions or exceptions to the standards and spacing with the Conway Corporation.

**5.3 INAPPLICABILITY OF CERTAIN ZONING REGULATIONS**

- 5.3.1 Within this TND Overlay District Ordinance, certain Regulations set forth in the Zoning Ordinance shall either not apply or shall be modified, as follows:
- 5.3.2 Lot, yard and height regulations in the T3 and the T4 Zones shall be those set forth in Article 4 and Tables 4 and 10 of this TND Overlay District Ordinance rather than those set forth in Zoning Ordinance Section 401.
- 5.3.3 Lot, yard and height regulations in the T5 and T6 Zones shall be those set forth in Article 4 and Tables 4 and 10 of this TND Overlay District Ordinance, rather than those set forth in Zoning Ordinance Section 401.
- 5.3.4 Development criteria for Commercial and Office space shall be those set forth in Article 4 and Article 6 of this TND Overlay District Ordinance, rather than those set forth in Section 401.5 of the Conway Zoning Ordinance.
- 5.3.5 Parking requirements, including but not limited to off-street parking, loading and parking area landscaping requirements shall be those set forth in Article 4 and Table 8 of this TND Overlay District Ordinance, rather than those set forth in Section 501 and Section 1101 of the Zoning Ordinance, Ordinance O-91-54 and Ordinance O-91-59.
- 5.3.6 Placement, size, and maximum rear yard coverage of Outbuildings or Ancillary Buildings shall be governed by Article 4 and Tables 4 and 10 of this TND Overlay District Ordinance, rather than any provision for accessory buildings set forth in the Zoning Ordinance, including Section 601.2.
- 5.3.7 The maximum building heights shall be as specified in Article 4 and Tables 4 and 10 of this TND Overlay District Ordinance. Therefore, any reference to height requirements in the Zoning Ordinance pertaining to occupied structures such as Section 401 or 601 of the Zoning Code shall not apply.
- 5.3.8 Architectural matters shall be governed by the Architectural Patternbook as stipulated in Article 4. Therefore the provisions of Section 1101 of the Zoning Ordinance varying the planes of exterior wall, varying the height of buildings so that they appear to be divided into distinct massing elements, articulating the different parts of a building's façade by use of color, arrangement of elements or change of materials, and incorporation of recurring, unifying and identifiable themes for the entire development site shall not apply.
- 5.3.9 Maximum impermeable surface shall be governed by Article 3 and Table 10 of this TND Overlay District Ordinance. Therefore, the provisions of Section 1101 of the Zoning Ordinance regarding maximum impermeable surface shall not apply.
- 5.3.10 The provisions of Section 1101.5-F of the Zoning Ordinance regarding Landscaping shall be governed by the Landscape Standards submitted with the preliminary plat, therefore the provisions of Section 1101 shall not apply.

- 5.3.11 The provisions of Section 1101 of the Zoning Ordinance regarding Traffic shall not apply.
- 5.3.12 The provisions of Section 1101 of the Zoning Ordinance regarding Other Requirements, including but not limited to parking lot ordinances, sign ordinances and access management ordinances shall not apply, unless specifically referred to in this TND Overlay District Ordinance.

**ARTICLE 6. STANDARDS AND TABLES**

The Tables and their associated metrics are an integral part of this TND Overlay Ordinance.

**TABLE 1: Transect Zone Descriptions.** This table provides description of the character of each Transect Zone.

<p><b>T1</b> THE NATURAL ZONE consists of lands approximating or reverting to a wilderness condition, including lands unsuitable for settlement due to topography, hydrology or vegetation.</p>	
<p><b>T2</b> THE RURAL ZONE consists of lands in open or cultivated state or sparsely settled. These include woodland, agricultural lands, grasslands and irrigable deserts.</p>	
<p><b>T3</b> THE SUB-URBAN ZONE, consists of low density suburban residential areas that allow home occupations. Planting is informal with setbacks relatively deep. Blocks may be large and the roads irregular to accommodate natural conditions.</p>	
<p><b>T4</b> THE GENERAL URBAN ZONE consists of a mixed-use but primarily residential urban fabric. It has a wide range of building types such as single, sideyard, and row houses. Setbacks and landscaping are variable. Streets typically define medium-sized blocks.</p>	
<p><b>T5</b> THE URBAN CENTER ZONE consists of higher density mixed-use building types that may accommodate retail, offices, rowhouses and apartments. It has a tight network of streets, with wide sidewalks, regular street tree planting and buildings set close to the frontages.</p>	
<p><b>T6</b> THE URBAN CORE ZONE consists of the highest density, with the greatest variety of uses, and civic buildings of regional importance. It may have larger blocks; streets have regular street tree planting and buildings set close to the frontages.</p>	

**TABLE 2A: Vehicular Lane Dimensions.** This table assigns lane widths to Transect Zones. The Design ADT (Average Daily Traffic) is the determinant for each of these sections. The most typical assemblies are shown in Table 2B. Specific requirements for truck and transit bus routes and truck loading shall be decided by Minor Exception.

DESIGN SPEED	TRAVEL LANE WIDTH	T1	T2	T3	T4	T5	T6
Below 20 mph	8 feet	■	■	■	■	□	□
20-25 mph	8-9 feet	■	■	■	■	■	■
25-35 mph	9-11 feet	■	■	□	□	■	■
Above 35 mph	12 feet	■	■	□	□	■	■

■ BY RIGHT

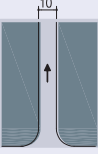
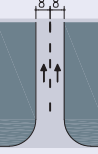

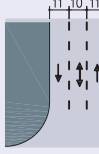
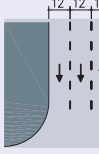
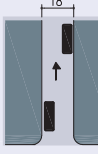
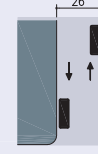
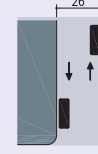
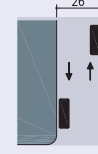
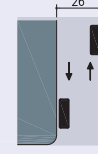
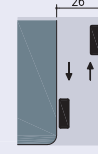
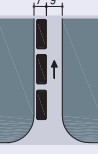
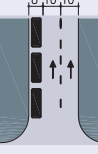
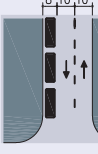



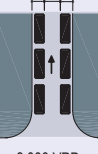
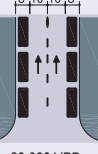




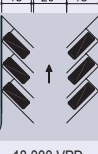

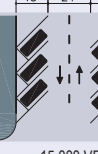
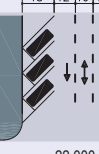
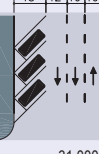

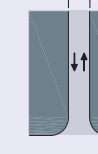
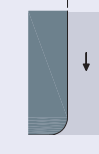
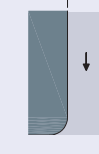
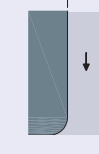
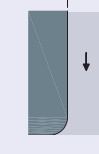
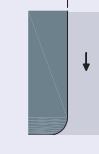
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DEDICATED							
DESIGN SPEED	PARKING LANE WIDTH						
20-25 mph	(Angle ) 18 feet					■	■
20-25 mph	(Parallel) 7 feet			■	■	■	■
25-35 mph	(Parallel) 8 feet			■	■	■	■
Above 35 mph	(Parallel) 9 feet					■	■

		(See Table 16b)					
DESIGN SPEED	CURB RADIUS						
Below 20 mph	2-5 feet			■	■	□	□
20-25 mph	2-15 feet			■	■	■	■
25-35 mph	10-20 feet	■	■	□	□	■	■
Above 35 mph	10 feet or more	■	■	□	□	■	■



**TABLE 2B: Vehicular Lane/Parking Assemblies.** The projected design speeds determine the dimensions of the vehicular lanes and turning radii assembled to create thoroughfares. Dimensions shown here are examples and may vary in implementation.

		ONE WAY MOVEMENT			TWO WAY MOVEMENT		
a. NO PARKING	NO PARKING	T1	T2	T3	T1	T2	T3
		T1	T2	T3	T1	T2	T3
							
Design AADT		300 VPD	600 VPD	2,500 VPD	22,000 VPD	36,000 VPD	
Pedestrian Crossing		3 Seconds	5 Seconds	5 Seconds	9 Seconds	13 Seconds	
Design Speed		20-30 MPH	Below 20 MPH	20-25 MPH	25-35 MPH	35 MPH and above	
b. PARKING BOTH SIDES PARALLEL ALTERNATE YIELD	PARKING BOTH SIDES PARALLEL ALTERNATE YIELD	T3	T4	T3	T4	T3	T4
		T3	T4	T3	T4	T3	T4
							
Design ADT		1,000 VPD	1,000 VPD	1,000 VPD	1,000 VPD	1,000 VPD	1,000 VPD
Pedestrian Crossing		5 Seconds	7 Seconds	7 Seconds	7 Seconds	7 Seconds	7 Seconds
Design Speed		Below 20 MPH	Below 20 MPH	Below 20 MPH	Below 20 MPH	Below 20 MPH	Below 20 MPH
c. PARKING ONE SIDE PARALLEL	PARKING ONE SIDE PARALLEL	T3	T4	T5	T6	T3	T4
		T3	T4	T5	T6	T3	T4
							
Design ADT		5,000 VPD	18,000 VPD	16,000 VPD	15,000 VPD	32,000 VPD	32,000 VPD
Pedestrian Crossing		5 Seconds	8 Seconds	8 Seconds	11 Seconds	13 Seconds	13 Seconds
Design Speed		20-30 MPH	25-30 MPH	25-30 MPH	25-30 MPH	35 MPH and above	35 MPH and above
d. PARKING BOTH SIDES PARALLEL	PARKING BOTH SIDES PARALLEL	T4	T5	T4	T5	T4	T5
		T4	T5	T4	T5	T4	T5
							
Design ADT		8,000 VPD	20,000 VPD	15,000 VPD	22,000 VPD	32,000 VPD	32,000 VPD
Pedestrian Crossing		7 Seconds	10 Seconds	10 Seconds	13 Seconds	15 Seconds	15 Seconds
Design Speed		Below 20 MPH	25-30 MPH	25-30 MPH	25-30 MPH	35 MPH and above	35 MPH and above
e. PARKING BOTH SIDES DIAGONAL	PARKING BOTH SIDES DIAGONAL	T5	T6	T5	T6	T5	T6
		T5	T6	T5	T6	T5	T6
							
Design ADT		18,000 VPD	20,000 VPD	15,000 VPD	22,000 VPD	31,000 VPD	31,000 VPD
Pedestrian Crossing		15 Seconds	17 Seconds	17 Seconds	20 Seconds	23 Seconds	23 Seconds
Design Speed		Below 20 MPH	20-25 MPH	20-25 MPH	25-30 MPH	25-30 MPH	25-30 MPH
f. PARKING ACCESS	PARKING ACCESS	T3	T4	T5	T5	T6	T5
		T3	T4	T5	T5	T6	T5
							
Design ADT		4 Seconds	8 Seconds	8 Seconds	8 Seconds	8 Seconds	8 Seconds
Pedestrian Crossing		Below 20 MPH	Below 20 MPH	Below 20 MPH	Below 20 MPH	Below 20 MPH	Below 20 MPH
Design Speed		Below 20 MPH	Below 20 MPH	Below 20 MPH	Below 20 MPH	Below 20 MPH	Below 20 MPH

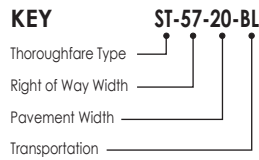
**TABLE 2C: Thoroughfare Assemblies.** These thoroughfares are assembled from the elements that appear in Tables 2A and 2B and incorporate the Public Frontages from Table 3. The key gives the thoroughfare type followed by the right-of-way width, followed by the pavement width, and in some instances followed by specialized transportation capability. Dimensions shown in the examples may vary within given ranges in implementation.

### THOROUGHFARE STANDARDS

Thoroughfare Type
Transect Zone Assignment
Right-of-Way Width
Pavement Width
Capacity
Movement
Design Speed
Pedestrian Crossing Time
Traffic Lanes
Parking Lanes
Curb Radius
Walkway Type
Planter Type
Curb Type
Landscape Type
Transportation Provision

LA-XX-XX
Lane
T3 & T4
8-24 feet
8-12 feet
Very Low Capacity
Yield Movement
8-10 MPH
2.5-3.5 seconds
1-2 lanes
None
2-10 feet
None
None
Inverted Crown
None
None

AL-XX-XX
Alley
T5 & T6
12-24 feet
12-24 feet
Very Low Capacity
Yield or Slow Movement
8-10 MPH
2.5-6.5 seconds
1-2 lanes
None
2-10 feet
None
None
Inverted Crown
None
None

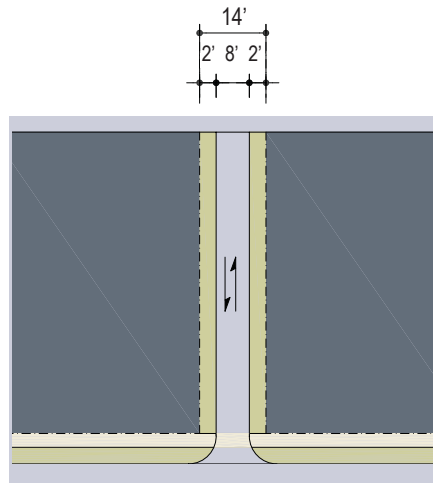


### THOROUGHFARE TYPES

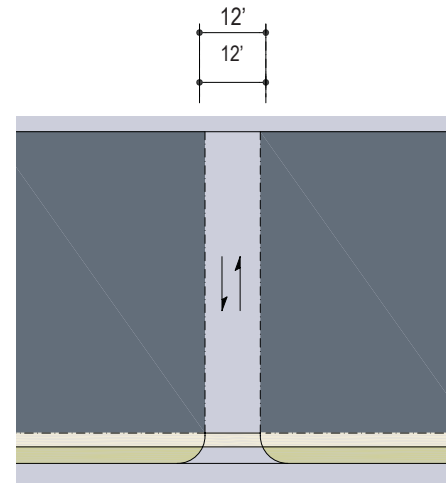
Boulevard:	BV
Avenue:	AV
Urban Street:	US
Street:	ST
Road:	RD
Alley:	AL
Lane:	LA
Bicycle Path:	BP
Bicycle Lane:	BL
Bicycle Route:	BR
Path:	PT
Transit Route:	TR

### THOROUGHFARE EXAMPLES

Example: LA-14-8



Example: AL-12-12

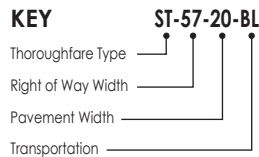


**TABLE 2C: Thoroughfare Assemblies.** These thoroughfares are assembled from the elements that appear in Tables 2A and 2B and incorporate the Public Frontages from Table 3. The key gives the thoroughfare type followed by the right-of-way width, followed by the pavement width, and in some instances followed by specialized transportation capability. Dimensions shown in the examples may vary within given ranges in implementation.

## THOROUGHFARE STANDARDS

	RD-XX-XX	ST-XX-XX
Thoroughfare Type	Road	Street
Transect Zone Assignment	T1, T2, T3, T4	T3, T4, T5
Right-of-Way Width	30-60 feet	40-60 feet
Pavement Width	14-24 feet	18-36 feet
Capacity	Low Capacity	Low Capacity
Movement	Slow Movement	Slow or Yield Movement
Design Speed	15-20 MPH	20-25 MPH
Pedestrian Crossing Time	4-7 seconds	7-10 seconds
Traffic Lanes	2 lanes	1-2 lanes
Parking Lanes	None	Parallel One or Both Sides
Curb Radius	10-25 feet	5-15 feet
Walkway Type	4-8 ft. Sidewalk or Path optional	4-15 foot Sidewalk
Planter Type	Continuous planter or swale	Continuous planter or Tree Well
Curb Type	Curb or Swale	Curb
Landscape Type	Trees clustered at 40' o.c. Avg.	Trees at 40' o.c. Avg.
Transportation Provision	BR	BR

## KEY

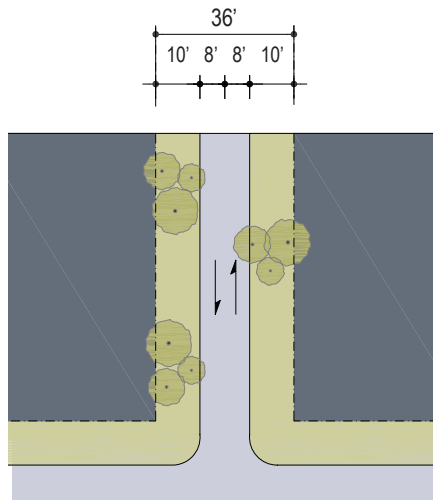


## THOROUGHFARE TYPES

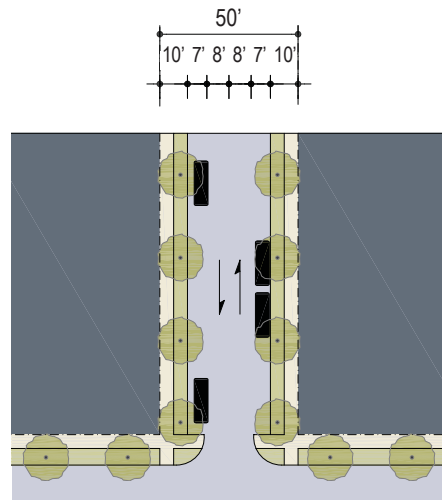
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Avenue:	AV
Urban Street:	US
Street:	ST
Road:	RD
Alley:	AL
Lane:	LA
Bicycle Path:	BP
Bicycle Lane:	BL
Bicycle Route:	BR
Path:	PT
Transit Route:	TR

## THOROUGHFARE EXAMPLES

Example: RD-36-16



Example: ST-50-30

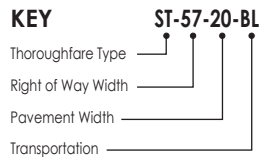


**TABLE 2C: Thoroughfare Assemblies.** These thoroughfares are assembled from the elements that appear in Tables 2A and 2B and incorporate the Public Frontages from Table 3. The key gives the thoroughfare type followed by the right-of-way width, followed by the pavement width, and in some instances followed by specialized transportation capability. Dimensions shown in the examples may vary within given ranges in implementation.

## THOROUGHFARE STANDARDS

	US-XX-XX	AV-XX-XX
Thoroughfare Type	Urban Street	Avenue
Transect Zone Assignment	T5, T6	T3, T4, T5, T6
Right-of-Way Width	40-100 feet	60-90 feet
Pavement Width	20-80 feet	32-56 feet
Capacity	High Capacity	High Capacity
Movement	Slow Movement	Slow Movement
Design Speed	25 MPH	25 MPH
Pedestrian Crossing Time	8-12 seconds	12-16 seconds
Traffic Lanes	1-2 lanes	2-4 lanes
Parking Lanes	One or Both Sides, parallel or angle	Parallel Both Sides
Curb Radius	5-15 feet	5-15 feet
Walkway Type	12-24 foot Sidewalk	6-24 foot Sidewalk
Planter Type	Tree well	5-15 foot Continuous planter
Curb Type	Curb	Curb
Landscape Type	Trees at 40' o.c. Avg. optional	Trees at 40' o.c. Avg.
Transportation Provision	BL	BR, TR

## KEY

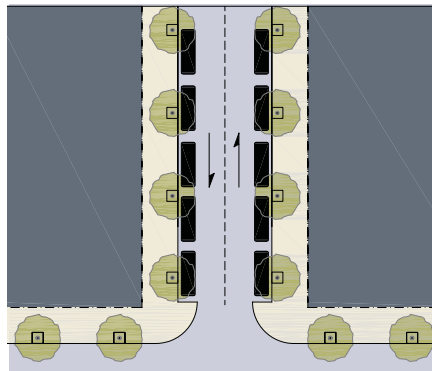
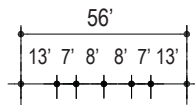


## THOROUGHFARE TYPES

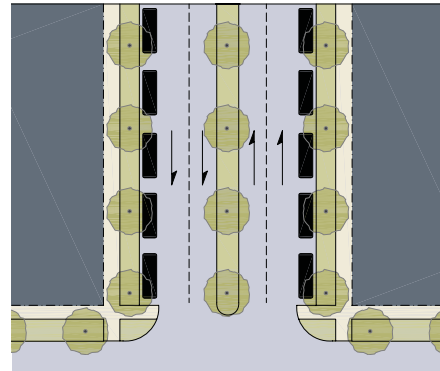
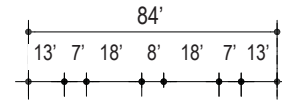
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Avenue:	AV
Urban Street:	US
Street:	ST
Road:	RD
Alley:	AL
Lane:	LA
Bicycle Path:	BP
Bicycle Lane:	BL
Bicycle Route:	BR
Path:	PT
Transit Route:	TR

## THOROUGHFARE EXAMPLES

Example: US-56-30



Example: AV-84-50

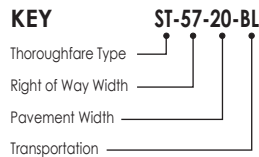


**TABLE 2C: Thoroughfare Assemblies.** These thoroughfares are assembled from the elements that appear in Tables 2A and 2B and incorporate the Public Frontages from Table 3. The key gives the thoroughfare type followed by the right-of-way width, followed by the pavement width, and in some instances followed by specialized transportation capability. Dimensions shown in the examples may vary within given ranges in implementation.

## THOROUGHFARE STANDARDS

	BV-XX-XX
Thoroughfare Type	Boulevard
Transect Zone Assignment	T5, T6
Right-of-Way Width	100-135 feet
Pavement Width	20 feet - 43 feet - 20 feet (average)
Capacity	high capacity
Movement	Free Movement
Design Speed	35 MPH
Pedestrian Crossing Time	6 seconds - 13 seconds - 6 seconds (average)
Traffic Lanes	4 lanes & optional two one-way slip roads
Parking Lanes	Parallel both sides optional
Curb Radius	5-15 feet
Walkway Type	5-15 foot Sidewalk
Planter Type	5-15 foot Continuous planter
Curb Type	Curb
Landscape Type	Trees at 40' o.c. Avg.
Transportation Provision	BR, TR

## KEY

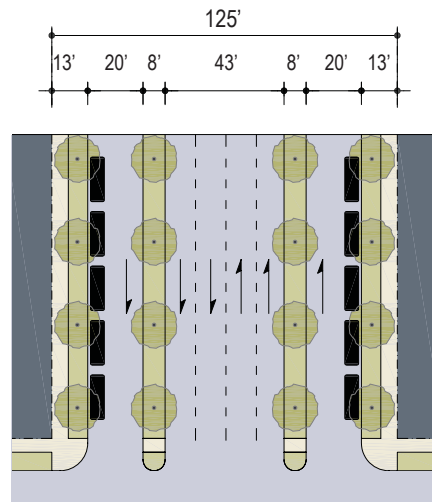


## THOROUGHFARE TYPES

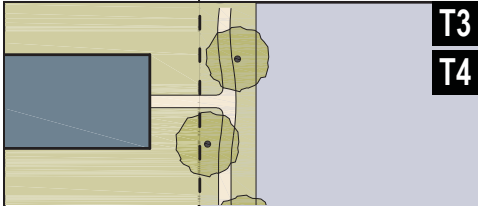
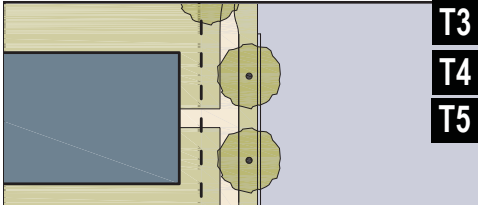
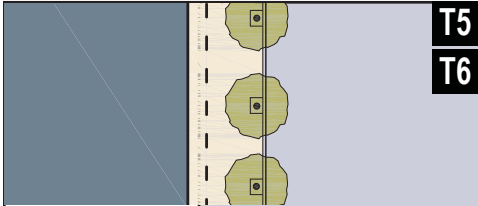
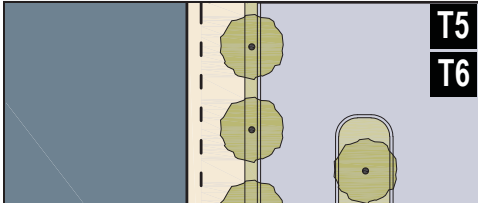
Boulevard:	BV
Avenue:	AV
Urban Street:	US
Street:	ST
Road:	RD
Alley:	AL
Lane:	LA
Bicycle Path:	BP
Bicycle Lane:	BL
Bicycle Route:	BR
Path:	PT
Transit Route:	TR

## THOROUGHFARE EXAMPLES

Example: BV-125-43



**TABLE 3A: Public Frontages - General.** The Public Frontage is the area between the private lot line and the edge of the vehicular lanes. Dimensions are given in Table 3B (Public Frontages - Specific)

PLAN	
PRIVATE FRONTAGE ►	LOT R.O.W. ◄ PUBLIC FRONTAGE
<p><b>a. (RD) For Roads:</b> This frontage may have curb &amp; gutter or open swales drained by percolation and sidewalks or a walking path or bicycle trail along one or both sides. Yield parking is optional. The landscaping consists of trees in a clustered or regular spaced pattern.</p>	 <div>T3</div> <div>T4</div>
<p><b>b. (ST) (AV) For Street or Avenues:</b> This frontage has raised curbs drained by inlets and narrow sidewalks separated from the vehicular lanes by a wide continuous planter, with parking on one or both sides. The landscaping consists of street trees aligned in a regularly spaced pattern.</p>	 <div>T3</div> <div>T4</div> <div>T5</div>
<p><b>c. (US) (AV) For Urban Streets or Avenues:</b> This frontage has raised curbs drained by inlets; wide sidewalks along both sides separated from vehicular lanes by tree wells; and parking on both sides. The landscaping may consist of trees aligned in a regularly spaced pattern where possible. This public frontage may be encroached upon by building awnings or arcades.</p>	 <div>T5</div> <div>T6</div>
<p><b>d. (BV) For Boulevards:</b> This frontage often has slip roads on both sides. It consists of raised curbs drained by inlets and sidewalks along both sides, separated from the vehicular lanes by planters. The landscaping consists of double rows of street trees aligned in a regularly spaced pattern.</p>	 <div>T5</div> <div>T6</div>



**TABLE 3B PUBLIC FRONTAGES-SPECIFIC**

**Table 3B: Public Frontages - Specific.** This table assembles prescriptions and dimensions for the public frontage elements - curbs, walkways and planters – relative to specific thoroughfare types within Transect Zones. Table 2C assembles all of the elements for the various street types.

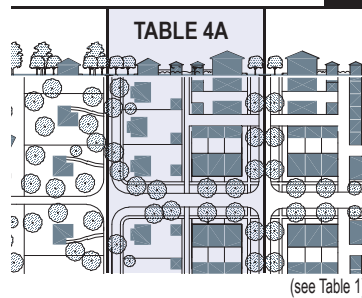
	R U R A L   T R A N S E C T   U R B A N					
	T1   T2   T3	T2   T3   T4	T3   T4	T4   T5	T5   T6	T5   T6
	RD	RD	ST	US-AV	US-AV-BV	US-AV-BV
<b>a. Assembly:</b> The principal variables are the type and dimension of curbs, walkways, planters and landscape.						
Total Width	16-24 feet	10-24 feet	7-20 feet	7-20 feet	11-28 feet	13-36 feet
<b>b. Curb:</b> The detailing of the edge of the vehicular pavement, incorporating drainage.						
Type	Open Swale	Open Swale	Raised Curb	Raised Curb	Raised Curb	Raised Curb
Radius	10-30 feet	10-30 feet	2-15 feet	2-20 feet	2-25 feet	2-25 feet
<b>c. Walkway:</b> The pavement dedicated exclusively to pedestrian activity.						
Type	Path Optional	Path	Sidewalk	Sidewalk	Sidewalk	Sidewalk
Width	n/a	4-8 feet	4-8 feet	4-8 feet	8-20 feet	10-30 feet
<b>d. Planter:</b> The layer which accommodates street trees and other landscape.						
Arrangement	Clustered	Clustered	Regular	Regular	Regular	Semi-Regular
Planter Type	Continuous Swale	Continuous Swale	Continuous Planter	Continuous Planter	Continuous Planter	Tree Well
Planter Width	8 feet-16 feet	6 feet-16 feet	3 feet-12 feet	3 feet-12 feet	3 feet-8 feet	3 feet-6 feet

# TND OVERLAY

City of Conway, Arkansas

# TABLE 4 BUILDING SCALE PLANS

T3

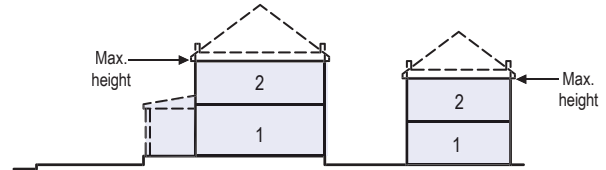


RESIDENTIAL DENSITY	
6 units per acre	
BUILDING HEIGHT (see Table 10)	
a. Principal Building	25 ft. to eave
b. Outbuilding	2 stories max.
LOT OCCUPATION	
a. Lot Width	50 ft. min
b. Lot Coverage	60% max
PRINCIPAL BUILDING SETBACK	
a. Front Setback	20 ft. min.
b. Side Street Setback	8 ft. min.
c. Side Setback	6 ft. min.
d. Rear Setback	12 ft. min.
OUTBUILDING SETBACK	
a. Front Setback	Prin. Bldg + 20 ft. min.
b. Side Street Setback	8 ft. min.
c. Side Setback	3 ft. min.
d. Rear Setback	6 ft. min.*
PRIVATE FRONTAGES (see Table 5)	
a. Common Lawn	permitted
b. Porch & Yard	permitted
c. Terrace or L.C.	prohibited
d. Forecourt	prohibited
e. Stoop	prohibited
f. Shopfront & Awning	prohibited
g. Gallery	prohibited
h. Arcade	prohibited
BUILDING TYPE (see Table 6)	
a. Edgeyard	permitted
b. Sideyard	prohibited
c. Rearyard	prohibited
d. Courtyard	prohibited
Refer to Summary Table 10	
PARKING PROVISIONS	
See Table 8	

\*Setback may be reduced to 2 ft. where an alley is provided.

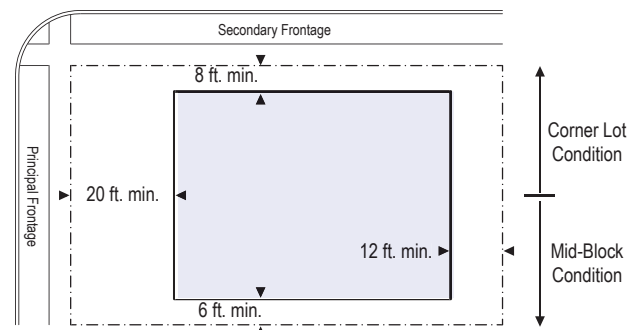
## BUILDING HEIGHT

1. Building height shall be measured in number of stories, excluding a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling.
3. Maximum height shall be measured to the eave or roof deck.



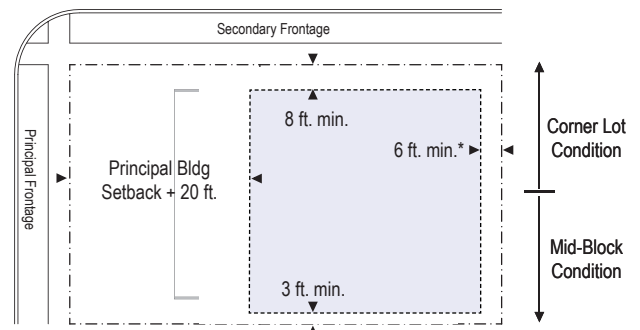
## BUILDING DISPOSITION

1. The facades and elevations of principal buildings shall be distanced from the lot lines as shown.



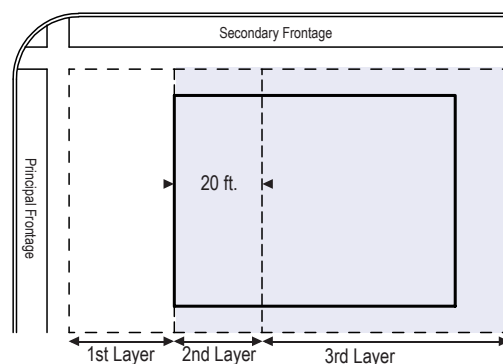
## OUTBUILDING DISPOSITION

1. The Outbuilding Setbacks shall be distanced from the lot lines as shown.
2. The Outbuilding Front Setback shall be the sum of the distance between the Principal Building and the lot line plus an additional 20 feet.



## PARKING PLACEMENT

1. Uncovered parking spaces may be provided within the 2nd and 3rd Layer as shown in the diagram (see Table 11D).
2. Covered parking shall be provided within the 3rd Layer as shown in the diagram (see Table 11D).
3. Trash containers shall be stored within the 3rd Layer.

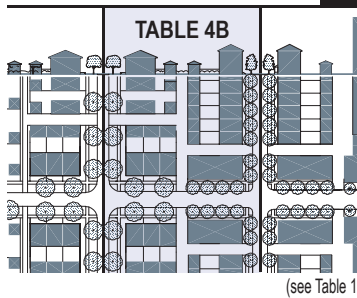


# TND OVERLAY

City of Conway, Arkansas

## TABLE 4 BUILDING SCALE PLANS

T4



### RESIDENTIAL DENSITY

12 units per acre

### BUILDING HEIGHT (see Table 10)

a. Principal Building 3 stories max.

b. Outbuilding 2 stories max.

### LOT OCCUPATION

a. Lot Width 18 ft. min.

b. Lot Coverage 70% max.

### PRINCIPAL BUILDING SETBACK

a. Front Setback 6 ft. min. 20 ft. max.

b. Side Street Setback 4 ft. min.

c. Side Setback 0 ft. min.

d. Rear Setback 3 ft. min.\*

### OUTBUILDING SETBACK

a. Front Setback 20 ft. min. + bldg. setback

b. Side Street Setback 3 ft. min.

c. Side Setback 0 ft. min.

d. Rear Setback 2 ft. min.

### PRIVATE FRONTAGES (see Table 5)

a. Common Lawn prohibited

b. Porch & Yard permitted

c. Terrace or L.C. permitted

d. Forecourt permitted

e. Stoop permitted

f. Shopfront & Awning permitted

g. Gallery permitted

h. Arcade prohibited

i. Flag Lot permitted

### BUILDING TYPE (see Table 6)

a. Edgeyard permitted

b. Sideyard permitted

c. Rearyard permitted

d. Courtyard permitted

\*Setback may be reduced to 2 ft. where an alley is provided.

\*\*See Table 5 Private Frontages.

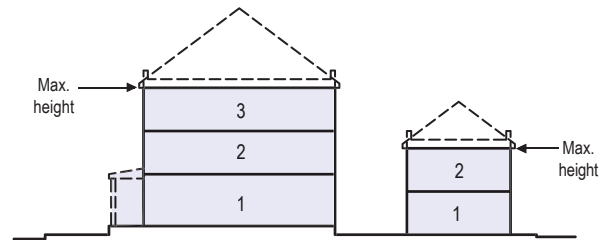
Refer to Summary Table 10

### PARKING PROVISIONS

See Table 8

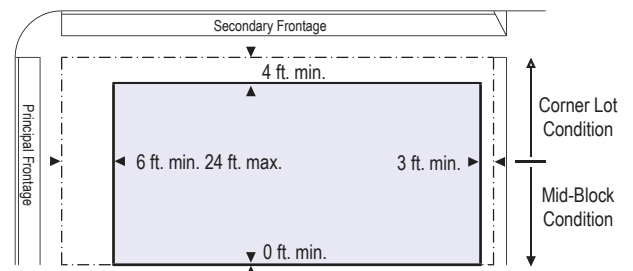
### BUILDING HEIGHT

1. Building height shall be measured in number of stories, excluding a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling.
3. Maximum height shall be measured to the eave or roof deck.



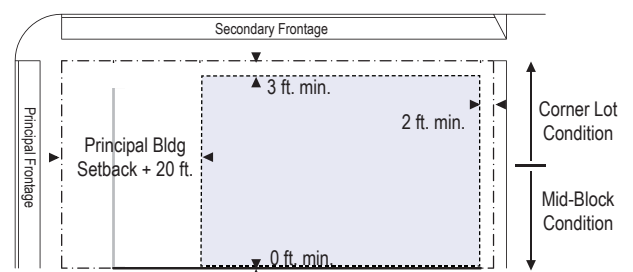
### BUILDING DISPOSITION

1. The facades and elevations of principal buildings shall be distanced from the lot lines as shown.
2. Buildings shall have facades along principal frontage lines and elevations along lot lines. (see Table 11E).



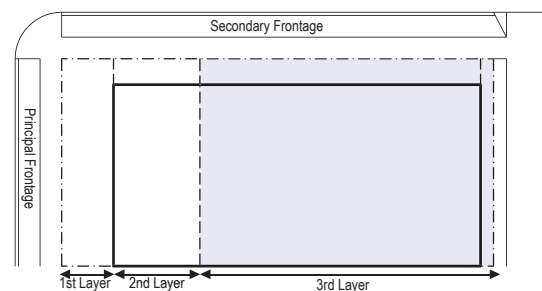
### OUTBUILDING PLACEMENT

1. The Outbuilding Setbacks shall be distanced from the lot lines as shown.
2. The Outbuilding Front Setback shall be the sum of the distance between the Principal Building and the lot line plus an additional 20 feet.



### PARKING PROVISIONS

1. Uncovered parking spaces may be provided within the 3rd Layer as shown in the diagram (see Table 11D).
2. Covered parking shall be provided within the 3rd Layer as shown in the diagram (see Table 11D).
3. Trash containers shall be stored within the 3rd Layer.

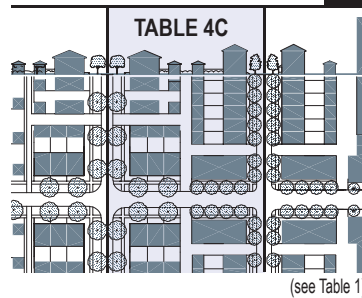


# TND OVERLAY

City of Conway, Arkansas

# TABLE 4 BUILDING SCALE PLANS

T5



## RESIDENTIAL DENSITY

24 units per acre

## BUILDING HEIGHT (see Table 10)

- a. Principal Building | 4 stories max. 2 min.
- b. Outbuilding | 2 stories max.

## LOT OCCUPATION

- a. Lot Width | 18 ft. min.
- b. Lot Coverage | 80% max.

## PRINCIPAL BUILDING SETBACK

- a. Front Setback | 0 ft. min. 12 ft. max.
- b. Side Street Setback | 3 ft. min. 18 ft. max.
- c. Side Setback | 0 ft. min. 24 ft. max.
- d. Rear Setback | 3 ft. min.

## OUTBUILDING SETBACK

- a. Front Setback | Prin. Bldg + 20 ft. min.
- b. Side Street Setback | 3 ft. min.
- c. Side Setback | 0 ft. min.
- d. Rear Setback | 2 ft. min.

## PRIVATE FRONTAGES (see Table 5)

- a. Common Lawn | prohibited
- b. Porch & Yard | prohibited
- c. Terrace or L.C. | permitted
- d. Forecourt | permitted
- e. Stoop | permitted
- f. Shopfront & Awning | permitted
- g. Gallery | permitted
- h. Arcade | permitted

## BUILDING TYPE (see Table 6)

- a. Edgeyard | prohibited
- b. Sideyard | permitted
- c. Rearyard | permitted
- d. Courtyard | permitted

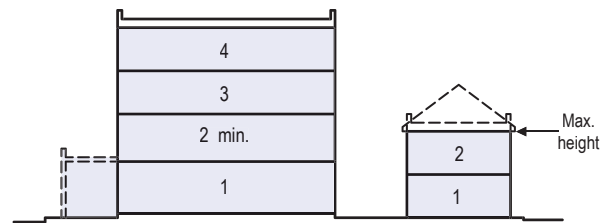
Refer to Summary Table 10

## PARKING PROVISIONS

See Table 8

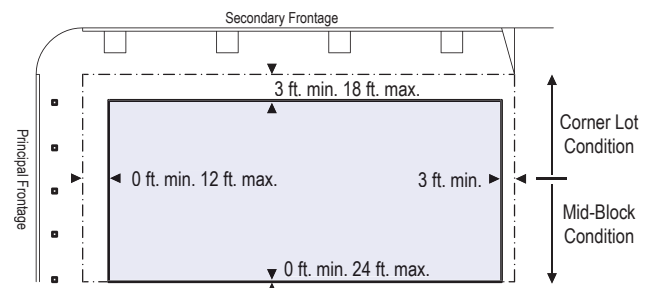
## BUILDING HEIGHT

1. Building height shall be measured in number of stories, excluding a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling.
3. Maximum height shall be measured to the eave or roof deck.



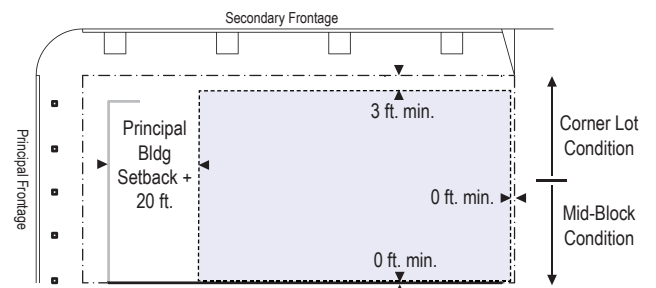
## BUILDING DISPOSITION

1. The facades and elevations of a building shall be distanced from the frontage and lot lines as shown.
2. Buildings shall have facades along the principal frontage lines and elevations along lot lines (see Table 11E).



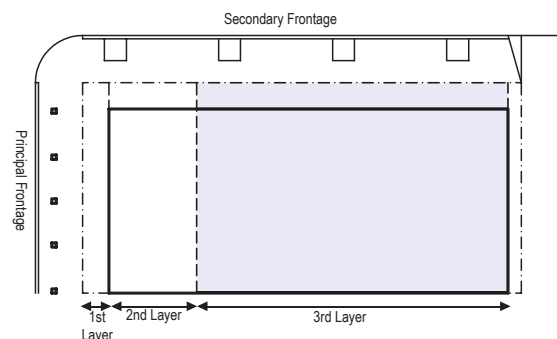
## OUTBUILDING DISPOSITION

1. The Outbuilding Setbacks shall be distanced from the lot lines as shown.
2. The Outbuilding Front Setback shall be the sum of the distance between the Principal Building and the lot line plus an additional 20 feet.



## PARKING PROVISIONS

1. Uncovered parking spaces may be provided within the 3rd Layer as shown in the diagram (see Table 11D).
2. Covered parking, if provided, shall be within the 3rd Layer as shown in the diagram (see Table 11D).
3. Trash containers shall be stored within the 3rd Layer as shown in the diagram (see Table 11D).

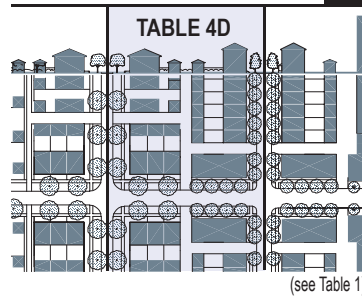


# TND OVERLAY

City of Conway, Arkansas

# TABLE 4 BUILDING SCALE PLANS

T6



(see Table 1)

## RESIDENTIAL DENSITY

60 units per acre

## BUILDING HEIGHT (see Table 10)

a. Principal Building | 6 stories max. 2 min.

b. Outbuilding | N/A

## LOT OCCUPATION

a. Lot Width | 18 ft. min.

b. Lot Coverage | 90% max.

## PRINCIPAL BUILDING SETBACK

a. Front Setback | 0 ft. min. 12 ft. max.

b. Side Street Setback | 0 ft. min. 18 ft. max.

b. Side Setback | 0 ft. min. 24 ft. max.

c. Rear Setback | 0 ft. min.

## OUTBUILDING SETBACK

a. Front | by Minor Exception

b. Side | by Minor Exception

c. Rear | by Minor Exception

## PRIVATE FRONTAGES (see Table 5)

a. Common Lawn | prohibited

b. Porch & Fence | prohibited

c. Terrace or L.C. | prohibited

d. Forecourt | permitted

e. Stoop | permitted

f. Shopfront & Awning | permitted

g. Gallery | permitted

h. Arcade | permitted

## BUILDING TYPE (see Table 6)

a. Edgeyard | prohibited

b. Sideyard | prohibited

c. Rearyard | permitted

d. Courtyard | permitted

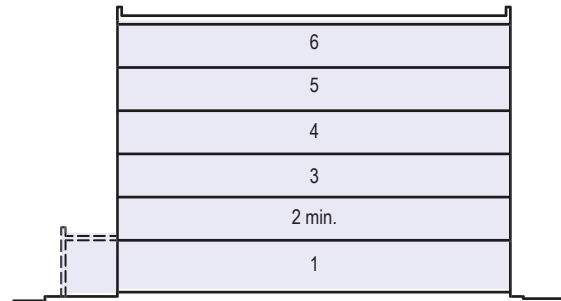
Refer to Summary Table 10

## PARKING PROVISIONS

See Table 8

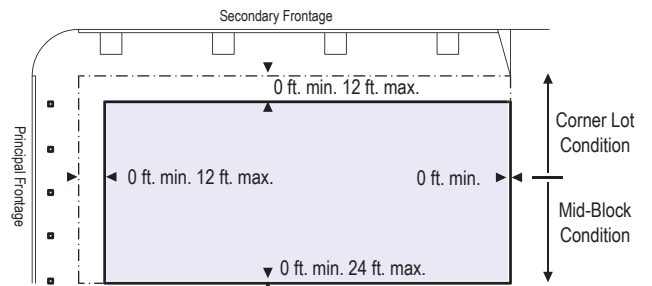
## BUILDING HEIGHT

1. Building height shall be measured in number of stories, not including a raised basement, or inhabited attic.
2. Each story shall not exceed 14 ft. clear, floor to ceiling. Check desired heights for retail bays.
3. Maximum height shall be measured to the eave or roof deck.



## BUILDING DISPOSITION

1. The facades and elevations of a building shall be distanced from the frontage and lot lines as shown.
2. Buildings shall have facades along frontage lines and elevations along lot lines (see Table 11E).
3. Outbuildings are only permitted by Minor Exception and shall follow the provisions in T5.



## PARKING PROVISIONS

1. Uncovered parking spaces may be provided within the 3rd Layer as shown in the diagram (see Table 11D).
2. Covered parking, if provided, shall be within the 2nd or 3rd Layer as shown in the diagram (see Table 11D).
3. Trash containers shall be stored within the 3rd Layer as shown in the diagram (see Table 11D).

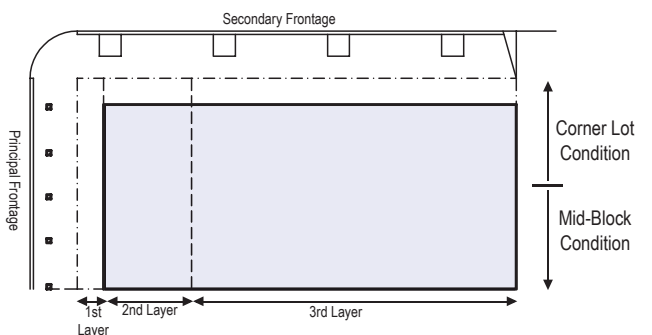
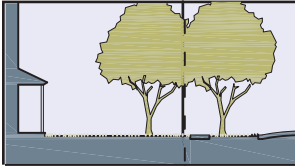
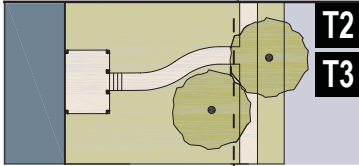
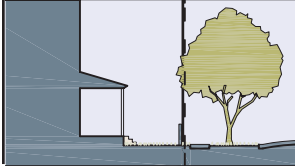
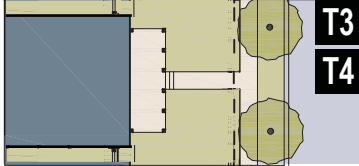
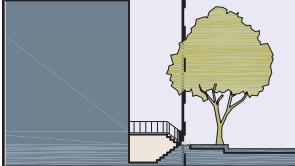
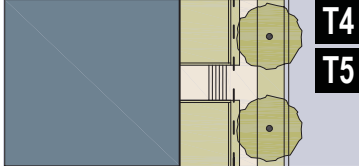

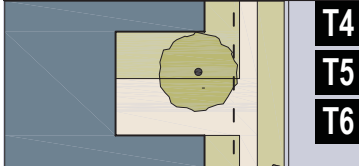

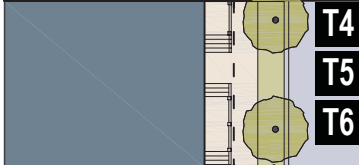
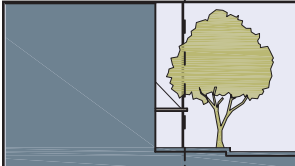

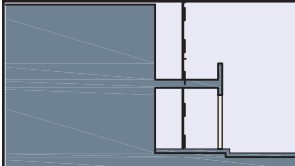

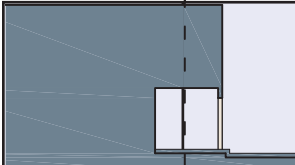


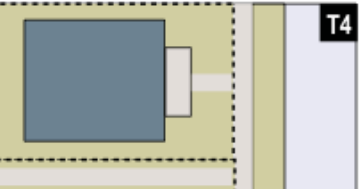
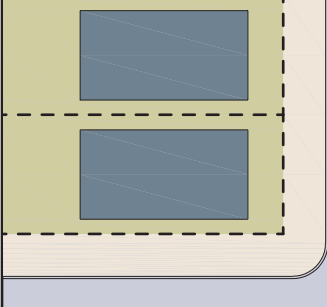
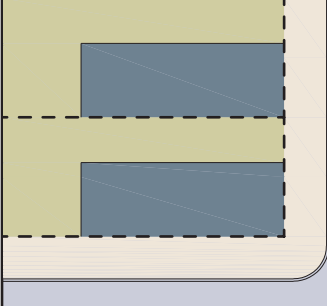
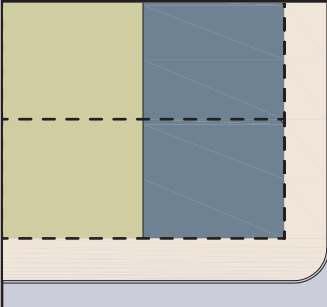
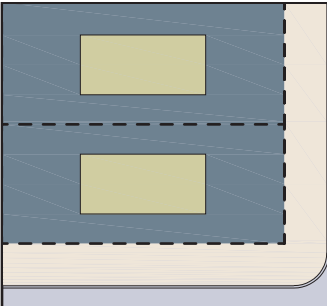
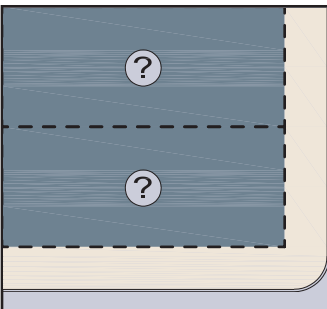


TABLE 5: Private Frontages. The Private Frontage is the area between the building and the lot lines.

	SECTION	PLAN
	LOT PRIVATE FRONTAGE R.O.W. PUBLIC FRONTAGE	LOT PRIVATE FRONTAGE R.O.W. PUBLIC FRONTAGE
a. <b>Common Yard:</b> a frontage wherein the facade is set back substantially from the frontage line. The front yard created remains unfenced and is visually continuous with adjacent yards, supporting a common landscape. The deep setback may provide a buffer from the higher speed thoroughfares.		 T2 T3
b. <b>Porch &amp; Yard:</b> a frontage wherein the facade is set back from the frontage line. An attached porch may encroach upon the setback. A fence should be constructed between the sides of adjacent buildings. The porches should be no less than 8 feet deep.		 T3 T4
c. <b>Terrace or Light Court:</b> a frontage wherein the facade is set back from the frontage line by an elevated terrace or a sunken light court. This type buffers residential use from urban sidewalks and removes the private yard from public access. The terrace is suitable for conversion to outdoor cafes.		 T4 T5
d. <b>Forecourt:</b> a frontage wherein a portion of the facade is close to the frontage line and the central portion is set back. The forecourt created can be suitable for vehicular drop-offs. This type should be allocated in conjunction with other frontage types. Large trees within the forecourts may overhang the sidewalks.		 T4 T5 T6
e. <b>Stoop:</b> a frontage wherein the facade is aligned close to the frontage line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor residential use.		 T4 T5 T6
f. <b>Shopfront and Awning:</b> a frontage wherein the facade is aligned close to the frontage line with the building entrance at sidewalk grade. This type is conventional for retail use. It includes shopfront windows at the sidewalk level and awnings that may overhang the sidewalk area.		 T4 T5 T6
g. <b>Gallery:</b> a frontage wherein the facade is aligned close to the frontage line with an attached cantilevered shed or a lightweight colonnade overhanging the sidewalk. This type is conventional for retail use. The gallery should be no less than 10 feet wide and may overlap the whole width of the sidewalk to within 2 feet of the curb.		 T4 T5 T6
h. <b>Arcade:</b> a frontage wherein the facade is a colonnade that overlaps the sidewalk, while the facade at sidewalk level remains at the frontage line. This type is conventional for retail use. The arcade should be no less than 12 feet wide and may overlap the whole width of the sidewalk to within 2 feet of the curb.		 T5 T6
i. <b>Flag Lot:</b> a frontage wherein one building is located to the rear of another which fronts directly on a thoroughfare. The lot is shaped like a flag, where a portion of the lot, between 3 and 15 ft. wide, fronts onto a thoroughfare, providing for pedestrian access and/or utility connections. A flag lot may be located behind one layer of lots fronting a thoroughfare. The Principal Frontage of a flag lot is assigned as the rear lot line of the lot between the flag and the thoroughfare. All lot coverage and setback requirements remain.		 T4



**TABLE 6:** Building Type. This table approximates the location of the structure relative to the boundaries of each individual lot, establishing suitable basic building types for each Transect Zone.

<p><b>a. Edgeyard:</b> A building that occupies the center of its lot with Setbacks on all sides. This is the least urban of types as the front yard sets it back from the frontage, while the side yards weaken the spatial definition of the public Thoroughfare space. The front yard is intended to be visually continuous with the yards of adjacent buildings. The rear yard can be secured for privacy by fences and a well-placed Backbuilding and/or Outbuilding. Sample Types Include: Single family House, Cottage, Villa, Estate House, Urban Villa.</p>	 <div data-bbox="1409 327 1453 516"> <b>T1</b>  <b>T2</b>  <b>T3</b>  <b>T4</b> </div>
<p><b>b. Sideyard:</b> A building that occupies one side of the lot with the Setback to the other side. The visual opening of the side yard on the street frontage causes this building type to appear freestanding. A shallow frontage Setback defines a more urban condition. If the adjacent building is similar with a blank party wall, the yard can be quite private. This type permits systematic climatic orientation in response to the sun or the breeze. Sample Types Include: Charleston Single House, zero-lot-line house.</p>	 <div data-bbox="1409 653 1453 747"> <b>T4</b>  <b>T5</b> </div>
<p><b>c. Rearyard:</b> A building that occupies the full frontage, leaving the rear of the lot as the sole yard. This is a very urban type as the continuous Facade steadily defines the public Thoroughfare. The rear Elevations may be articulated for functional purposes. In its Residential form, this type is the Rowhouse. For its Commercial form, the rear yard can accommodate substantial parking. Sample Types Include: Townhouse, Rowhouse, Live-Work unit, perimeter block.</p>	 <div data-bbox="1409 978 1453 1115"> <b>T4</b>  <b>T5</b>  <b>T6</b> </div>
<p><b>d. Courtyard:</b> A building that occupies the boundaries of its lot while internally defining one or more private patios. This is the most urban of types, as it is able to shield the private realm from all sides while strongly defining the public Thoroughfare. Because of its ability to accommodate incompatible activities, masking them from all sides, it is recommended for workshops, Lodging and schools. Sample Types Include: Patio House.</p>	 <div data-bbox="1409 1304 1453 1398"> <b>T5</b>  <b>T6</b> </div>
<p><b>e. Specialized:</b> A building that is not subject to categorization. Buildings dedicated to manufacturing and transportation are often distorted by the trajectories of machinery. Civic buildings, which may express the aspirations of institutions, may be included.</p>	 <div data-bbox="1409 1629 1453 1682"> <b>SD</b> </div>

**TABLE 7:** Specific Function. The building functions permitted are specified according to Transect Zone below. Any functions not listed below are subject to Major or Minor Exception.

	T1	T2	T3	T4	T5	T6	SD
<b>a. RESIDENTIAL</b>							
Apartment Building				■	■	■	
Row House				■	■	■	
Duplex House				■	■		
Sideyard House			■	■	■		
Cottage			■	■			
House		■	■	■			
Estate House		■					
Accessory Unit		■	■	■	■		
Manufactured House			□				□
Temporary Tent	□	□	□	□	□	□	□
Live-Work Unit			■	■	■	■	□
<b>b. LODGING</b>							
Hotel (no room limit)					■	■	□
Inn (up to 12 rooms)		□		■	■	■	
Inn (up to 5 rooms)		□	■	■	■	■	
S.R.O. hostel			□	□	□	□	□
School Dormitory				■	■	■	■
<b>c. OFFICE</b>							
Office Building				■	■	■	□
Live-Work Unit			■	■	■	■	□
<b>d. RETAIL</b>							
Open-Market Building		■	■	■	■	■	■
Retail Building				■	■	■	□
Display Gallery				■	■	■	□
Restaurant				■	■	■	□
Kiosk				■	■	■	□
Push Cart					□	□	□
Private Club					■	■	■
<b>e. CIVIC</b>							
Bus Shelter			■	■	■	■	■
Convention Center						□	■
Conference Center					□	■	■
Exhibition Center						□	■
Fountain or Public Art		■	■	■	■	■	■
Library				■	■	■	■
Live or Movie Theater					■	■	■
Museum					□	■	■
Outdoor Auditorium		□	■		■	■	■
Parking Structure					■	■	■
Passenger Terminal					□	□	■
Playground		■	■	■	■	■	■
Sports Stadium						□	■
Surface Parking Lot				□	□	□	■
Religious Assembly					■	■	■
<b>f. OTHER: AGRICULTURE</b>							
Grain Storage		■	■				□
Livestock Pen	□	□					□
Greenhouse	■	■	□				□
Stable	■	■	□				□
Kennel	■	■	□	□	□	□	□
<b>f. OTHER: AUTOMOTIVE</b>							
Gasoline		□			□	□	■
Automobile Service							■
Truck Maintenance							■
Drive-Through Facility					□	□	■
Rest Stop	■	■					□
Roadside Stand	■	■					□
Shopping Center							□
Shopping Mall							□
<b>f. OTHER: CIVIL SUPPORT</b>							
Fire Station			■	■	■	■	■
Police Station				■	■	■	■
Cemetery		■	□	□			■
Funeral Home				■	■	■	■
Hospital					□	□	■
Medical Clinic				□	■	■	■
<b>f. OTHER: EDUCATION</b>							
College					□	□	■
High School				□	□	□	■
Trade School					□	□	■
Elementary School			□	■	■	■	■
Other- Childcare Center		■	■	■	■	■	□
<b>f. OTHER: INDUSTRIAL</b>							
Heavy Industrial Facility							■
Light Industrial Facility						□	■
Truck Depot							■
Laboratory Facility						□	■
Water Supply Facility							■
Sewer and Waste Facility							■
Electric Substation	□	□	□	□	□	□	■
Wireless Transmitter	□	□					■
Cremation Facility							■
Warehouse						□	■
Produce Storage							■
Mini-Storage							■

■ BY RIGHT

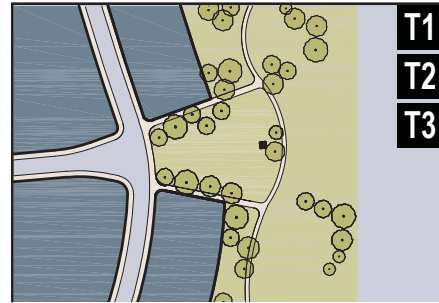
□ BY MINOR EXCEPTION

**TABLE 8: Parking Calculation.** The Required Parking table specifies the minimum amount of parking that must be provided on site. See Article 4.5 for more information.

a. REQUIRED PARKING			
	T2   T3	T4	T5   T6
RESIDENTIAL	up to 2.0	up to 2.0	1.0 / dwelling
LODGING	1.0 / bedroom	1.0 / bedroom	1.0 / bedroom
OFFICE	2.0 / 1000 sq. ft.	2.0 / 1000 sq. ft.	2.0 / 1000 sq. ft.
RETAIL	2.0 / 1000 sq. ft.	2.0 / 1000 sq. ft.	2.0 / 1000 sq. ft.
CIVIC	1 per four seats (assembly uses)		
OTHER	To be determined by minor exception		

b. SHARING FACTOR			
Function	with		Function
RESIDENTIAL			RESIDENTIAL
LODGING			LODGING
OFFICE		1	OFFICE
RETAIL		1.1	RETAIL
	1.4	1	
	1.2	1.7	
	1.3	1	
	1.2	1.3	
	1	1	

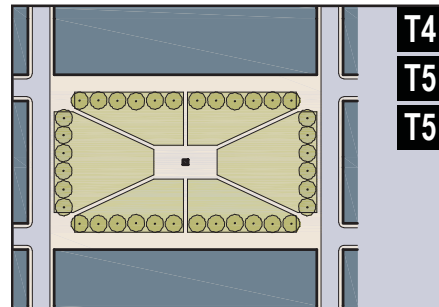
**a. Park:** A natural preserve available for unstructured recreation. A park may be independent of surrounding building frontages. Its landscape shall consist of paths and trails, meadows, woodland and open shelters, all naturalistically disposed. Parks may be lineal, following the trajectories of natural corridors.



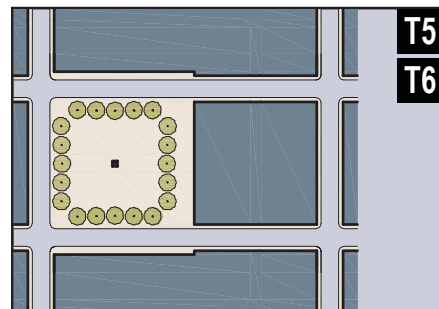
**b. Green:** An open space, available for unstructured recreation. A green may be spatially defined by landscaping rather than building frontages. Its landscape shall consist of lawn and trees, naturalistically disposed.



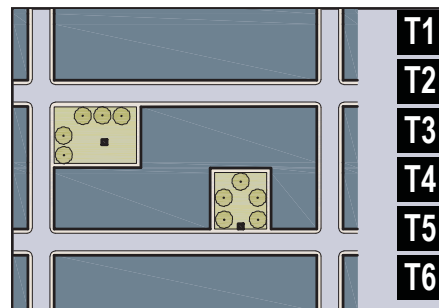
**c. Square:** An open space available for unstructured recreation and civic purposes. A square is spatially defined by building frontages. Its landscape shall consist of paths, lawns and trees, formally disposed. Squares shall be located at the intersection of important thoroughfares.



**d. Plaza:** An open space, available for civic purposes and commercial activities. A plaza shall be spatially defined by building frontages. Its landscape shall consist primarily of pavement. Trees are optional. Plazas shall be located at the intersection of important streets.



**e. Playground:** An open space designed and equipped for the recreation of children. A playground shall be fenced and may include an open shelter. Playgrounds shall be interspersed within residential areas and may be placed within a block. Playgrounds may be included within parks and greens. There should be no minimum or maximum size.



# TND OVERLAY

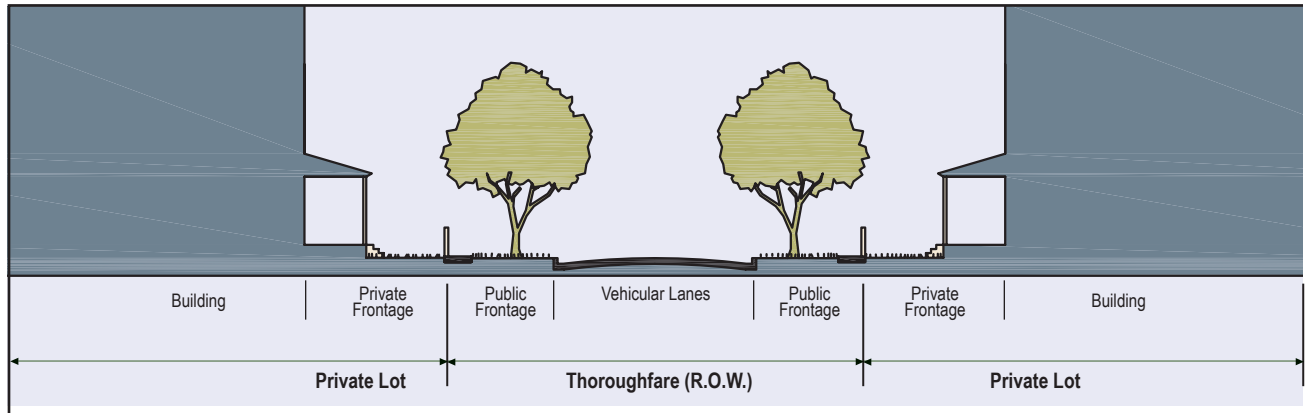
City of Conway, Arkansas

## TABLE 10 TND OVERLAY SUMMARY

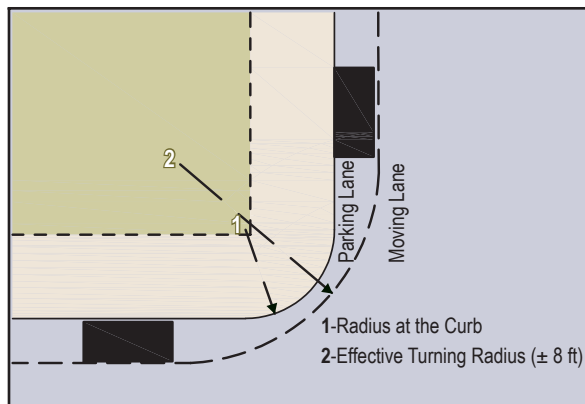
R U R A L   U R B A N							
	<b>T1</b>	<b>T2</b>	<b>T3</b>	<b>T4</b>	<b>T5</b>	<b>T6</b>	
	NATURAL ZONE	RURAL ZONE	SUB-URBAN ZONE	GENERAL URBAN ZONE	URBAN CENTER ZONE	URBAN CORE ZONE	
<b>A. BASE RESIDENTIAL DENSITY (see Section 3.4)</b>							
By Right	N/A	N/A	6 units / acre	12 units / acre	24 units / acre	60 units / acre	
Other Functions	N/A	N/A	80 - 100% min	60 - 80% min	40 - 60% min	less than 40%	
<b>B. BLOCK SIZE</b>							
Block Perimeter	N/A	N/A	2400 ft. max	2000 ft. max	1800 ft. max	2000 ft. max*	
<b>C. THOROUGHFARE/PUBLIC FRONTAGES (see Tables 2 and 3)</b>							
RD	N/A	N/A	permitted	permitted	prohibited	prohibited	
ST	N/A	N/A	permitted	permitted	permitted	prohibited	
US	N/A	N/A	prohibited	permitted	permitted	permitted	
AV	N/A	N/A	permitted	permitted	permitted	permitted	
BV	N/A	N/A	prohibited	prohibited	permitted	permitted	
Lane	N/A	N/A	permitted	permitted	prohibited	prohibited	
Alley	N/A	N/A	permitted	permitted	permitted	permitted	
Path	N/A	N/A	permitted	permitted	permitted	permitted	
Passage	N/A	N/A	permitted	permitted	permitted	permitted	
Bicycle Trail	N/A	N/A	prohibited	prohibited	permitted	permitted	
Bicycle Lane	N/A	N/A	prohibited	permitted	permitted	permitted	
Bicycle Route	N/A	N/A	permitted	permitted	permitted	permitted	
<b>D. CIVIC SPACES (see Table 9)</b>							
Park	N/A	N/A	permitted	prohibited	prohibited	prohibited	
Green	N/A	N/A	permitted	permitted	prohibited	prohibited	
Square	N/A	N/A	prohibited	permitted	permitted	permitted	
Plaza	N/A	N/A	prohibited	prohibited	permitted	permitted	
Playground	N/A	N/A	permitted	permitted	permitted	permitted	
<b>E. BUILDING HEIGHT (see Table 4)</b>							
Principal Building	N/A	N/A	25 ft. to eave	3 stories max.	4 stories max, 2 min	6 stories max, 2 min	
Outbuilding	N/A	N/A	2 stories max.	2 stories max	2 stories max	N/A	
<b>F. LOT OCCUPATION</b>							
Lot Width	N/A	N/A	50 ft. min 120 ft. max	18 ft. min 96 ft. max	18 ft. min 180 ft. max	18 ft. min	
Lot Coverage	N/A	N/A	60% max	70% max	80% max	90% max	
<b>G. BUILDING SETBACK</b>							
Front Setback	N/A	N/A	20 ft. min	6 ft. min 20 ft. max	0 ft. min 12 ft. max	0 ft. min 12 ft. max	
Secondary Front	N/A	N/A	8 ft. min.	4 ft. min.	3 ft. min 18 ft. max	0 ft. min 18 ft. max	
Side Setback	N/A	N/A	6 ft. min	0 ft. min	0 ft. min 24 ft. max	0 ft. min 24 ft. max	
Rear Setback	N/A	N/A	12 ft. min	3 ft. min.	3 ft. min.	0 ft. min.	
<b>H. OUTBUILDING SETBACK</b>							
Front Setback	N/A	N/A	Prin. bldg + 20 ft. min.	20 ft. min + bldg setback	Prin. bldg + 20 ft. min.	by Minor Exception	
Secondary Front	N/A	N/A	8 ft. min.	3 ft. min.	3 ft. min.	by Minor Exception	
Side Setback	N/A	N/A	3 ft. min	0 ft. min	0 ft. min	by Minor Exception	
Rear Setback	N/A	N/A	2 ft. min	2 ft. min.	2 ft. min.	by Minor Exception	
<b>I. PRIVATE FRONTAGES (see Table 5)</b>							
Common Yard	N/A	N/A	permitted	prohibited	prohibited	prohibited	
Porch & Fence	N/A	N/A	permitted	permitted	prohibited	prohibited	
Terrance or L.C.	N/A	N/A	prohibited	permitted	permitted	prohibited	
Forecourt	N/A	N/A	prohibited	permitted	permitted	permitted	
Stoop	N/A	N/A	prohibited	permitted	permitted	permitted	
Shopfront & Awning	N/A	N/A	prohibited	permitted	permitted	permitted	
Gallery	N/A	N/A	prohibited	prohibited	permitted	permitted	
Arcade	N/A	N/A	prohibited	prohibited	permitted	permitted	
Flag Lot	N/A	N/A	prohibited	permitted	prohibited	prohibited	
<b>J. BUILDING TYPE (see Table 9)</b>							
Edgeyard	N/A	N/A	permitted	permitted	prohibited	prohibited	
Sideyard	N/A	N/A	prohibited	permitted	permitted	prohibited	
Rearyard	N/A	N/A	prohibited	permitted	permitted	permitted	
Courtyard	N/A	N/A	prohibited	permitted	permitted	permitted	

\*3000 with interior parking deck

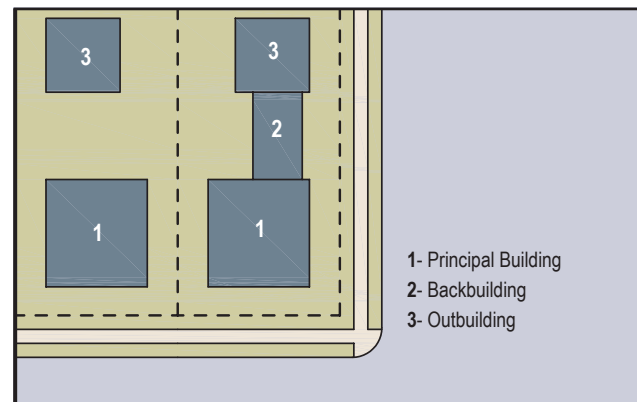
## a. THOROUGHFARE & FRONTAGES



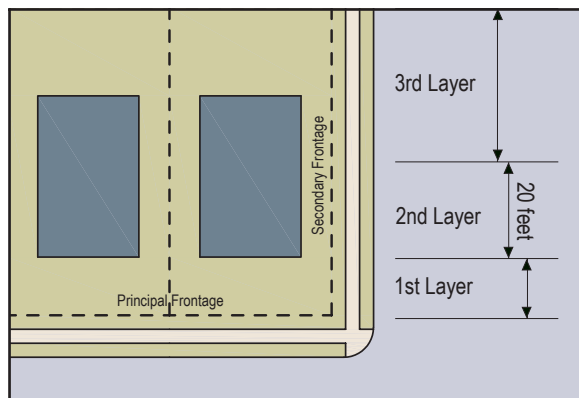
## b. TURNING RADIUS



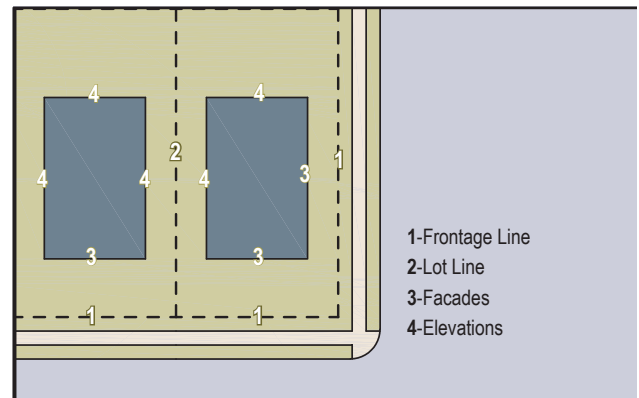
## c. BUILDING CONFIGURATION



## d. LOT LAYERS



## e. FRONTAGE & LOT LINES



## ARTICLE 7. DEFINITIONS OF TERMS

This Article provides definitions for terms in this Code that are technical in nature or that otherwise may not reflect a common usage of the term. If a term is not defined in this Article, then the Planning Office shall determine the correct definition of the term.

### DEFINITIONS

**Ancillary Unit:** an apartment not greater than 600 square feet sharing ownership and utility connections with a Principal Building. An Ancillary Unit may or may not be within an outbuilding. Ancillary Units do not count toward maximum density calculations (see Tables 4, 10 and 11).

**Apartment:** a dwelling unit sharing a building and a lot with other dwellings and/or uses. Apartments may be for rent or for sale as condominiums

**Avenue (AV):** a thoroughfare of high vehicular capacity and low speed. Avenues are short distance connectors between urban centers. Avenues typically have at least two travel lanes (one in each direction) and parallel parking. Avenues may be equipped with a landscaped median. The Public Frontage has curb, gutter and sidewalk. When located in T3 and T4, Avenues usually have trees located in planters and when located in T5 and T6, Avenues usually have tree wells and wider sidewalks. Avenues become collectors upon exiting urban areas.

**Backbuilding:** a single-story structure connecting a principal building to an outbuilding (see Table 11)

**Bicycle Lane (BL):** a dedicated bicycle lane running within a moderate-speed vehicular thoroughfare, demarcated by striping.

**Bicycle Route (BR):** a thoroughfare suitable for the shared use of bicycles and automobiles moving at low speeds.

**Bicycle Trail (BT):** a bicycle way running independently of a high-speed vehicular thoroughfare.

**Bill of Assurance:** a general description of any proposed covenants, restrictions and conditions applicable to the property included in the submitted plat.

**Block:** the aggregate of private lots, passages, rear lanes and alleys, circumscribed by thoroughfares.

**Boulevard (BV):** a thoroughfare designed for high vehicular capacity and moderate speed that is typically located in T5 and T6. Boulevards are long-distance thoroughfares that serve a city-wide function by connecting urbanized areas. Boulevards typically have at least four travel lanes (two in each direction) and may have parallel parking. Boulevards are usually equipped with slip roads that buffer the sidewalks and buildings or alternately may have a median. The Public Frontage has curb, gutter, sidewalk and trees located in either planters or tree wells. Boulevards become arterials upon exiting urban areas.

**Brownfield:** an area previously used primarily as an industrial site.

**Building Configuration:** the form of a building, based on its massing, private frontage, and height.

**Building Disposition:** the placement of a building on its lot (see Tables 4, 6 & 11).

**Building Function:** the uses accommodated by a building and its lot. (see Table 7).

**Building Height:** is measured as the vertical distance above the highest sidewalk elevation measured to the base of a parapet or roof line of a flat roof, the eave of a pitched roof or the deck line of a mansard roof. Where building heights are measured in stories, no story height shall exceed 15 feet. Height limits do not apply to masts, belfries, clock towers, chimney flues, water tanks, elevator bulkheads and similar structures. Building Height shall be measured from the average grade of the enfronting thoroughfare (see Table 4).

**Building Type:** a structure category determined by function, disposition on the lot, and configuration, including frontage and height.



**By Right Permit:** a proposal for a building or community plan that complies with this code and may thereby be processed administratively, without public hearing.

**Civic:** the term defining not-for-profit organizations dedicated to arts, culture, education, recreation, government, transit, and municipal parking.

**Civic Building:** a building designed specifically for a civic function.

**Civic Parking Reserve:** parking structure or lot within a quarter-mile of the site that it serves. Space may be leased or bought from this Reserve to satisfy parking requirements.

**Civic Space:** an outdoor area dedicated for public use. Civic Space types are defined by the combination of certain physical constants including the relationship between their intended use, their size, their landscaping and their enfronting buildings (see Table 9).

**Commercial:** the term collectively defining workplace, office and retail functions.

**Common Destination:** An area of focused community activity defining the approximate center of a Pedestrian Shed. It may include without limitation one or more of the following: a Civic Space, a Civic Building, a Commercial center, a bus stop. A Common Destination may act as the social center of a Neighborhood.

**Context:** surroundings made up of the particular combination of elements that create specific habitat.

**Corridor:** a lineal geographic system incorporating transportation and/or greenway trajectories. A transportation corridor may be a lineal urban Transect Zone.

**Cottage:** an edgeward building type. A single-family dwelling, on a regular lot, often shared with an ancillary building in the rearyard.

**Courtyard Building:** a building that occupies the boundaries of its lot while internally defining one or more private patios.

**Curb:** the edge of the vehicular pavement detailed as a raised curb or flush to a swale. The Curb usually incorporates the drainage system.

**Density:** the number of dwelling units within a standard measure of land area, usually given as units per acre.

**Design Review Board (DRB):** a board established by the developer that must approve all building plans prior to the issuance of a building permit and a Certificate of Occupancy.

**Design Speed:** is the velocity at which a thoroughfare tends to be driven without the constraints of signage or enforcement. There are three ranges of speed: Very Low: (below 20 MPH); Low: (20-25 MPH); Moderate: (25-35 MPH); High: (above 35 MPH). Lane width is determined by desired design speed.

**Development Review Committee (DRC):** Usually part of the Planning Department, a DRC is composed of one representative from each of the regulatory agencies that have jurisdiction of over the permitting of a project (Planning Department, Fire Department, Conway Corporation or public utility, Sanitation Department, and Street Department)

**Driveway:** a vehicular lane within a lot, usually leading to a garage. A Driveway in the First Layer may be used for parking if it is no more than 18 feet wide, thereby becoming subject to the constraints of a parking lot.

**Edgeward Building:** a building that occupies the center of its lot with setbacks on all sides.

**Elevation:** an exterior wall of a building not along a Frontage Line. See: **Facade**

**Enfront:** to place an element along a frontage line, as in "porches enfront the street."

**Entrance, Principal:** the main point of access of pedestrians into a building.

**Estate House** (Syn.: Country house, Villa): an edgeward building type. A single-family dwelling on a very large lot of rural character, often shared by one or more ancillary buildings.

**Exceptions:** See **Minor Exception; Major Exception)**

**Facade:** the exterior wall of a building that is set along a Frontage Line (see **Elevation**; **Frontage Line**).

**Frontage:** The area adjacent to the street. This includes the Public Frontage (the area within the Right of Way between the curb and the lot line) and the Private Frontage (the area of a private lot adjacent to the street).

**Frontage Line:** the lot lines that coincide with a public frontage. Facades along Frontage Lines define the public realm and are therefore more regulated than the elevations that coincide with other Lot Lines (see Table 11).

**GIS (Geographic Information System):** a computerized program in widespread municipal use that organizes data on maps. Various municipal departments can input information including the location of wetlands, thoroughfares, water/sewer lines, boundaries, building footprints, schools, zoning, land-use, etc. GIS makes information available as layered databases.

**Greenfield:** a project planned for an undeveloped area outside the existing urban fabric. See **Infill**.

**Greenway:** an open space corridor in largely natural conditions which may include Trails for bicycles and pedestrians.

**Greyfield:** an area previously used primarily as a parking lot. Shopping centers and shopping malls are typical Greyfield sites.

**Home Occupation:** non-retail commercial enterprises permitted in Zones T3-6. The work quarters should be invisible from the frontage, located either within the house or in an outbuilding.

**House** (Syn.: Single): an edgeyard building type. A single-family dwelling on a large lot, often shared with an ancillary building in the rearyard.

**Independent Building:** a building designed by a different architect from the adjacent buildings.

**Infill:** a project within existing urban fabric.

**Inside Turning Radius:** the curved edge of a thoroughfare at an intersection, measured at the inside edge of the vehicular tracking. The smaller the Turning Radius, the smaller the pedestrian crossing distance and the more slowly the vehicle is forced to make the turn.

**Long Pedestrian Shed:** A Pedestrian Shed of 1/2 mile radius used for mapping community types when a transit stop (bus or rail) is present or proposed as the Common Destination. People have been shown to walk ten minutes to transit. See **Pedestrian Shed**.

**Layer:** a range of depth of a lot within which certain elements are permitted.

**Liner Building:** a building specifically designed to mask a parking lot or a parking garage from a frontage. A Liner Building, if less than 30 feet deep and two stories, shall be exempt from parking requirements.

**Live-Work:** a fee-simple dwelling unit that contains a Commercial component anywhere in the unit. (Syn.: Flexhouse.) (See **Work-Live**.)

**Lodging:** premises available for daily and weekly renting of bedrooms. The area allocated for food service shall be calculated and provided with parking according to retail use.

**Lot Line:** the boundary that legally and geometrically demarcates a lot (see **Frontage Line**). Such lines appear graphically on Community and Site Plans. Codes reference lot lines as the baseline for measuring setbacks.

**Lot Width:** the length of the principal Frontage Line of a lot.

**Major Exception:** are major departures from the TND Overlay or the approved TND Plan. A Major Exception is any ruling on a deviation other than a Minor Exception. Major Exceptions shall be granted by the Conway Planning Department and subject to review by the Planning Commission and City Council.

**Manufacturing:** premises available for the creation, assemblage and/or repair of artifacts, using table-mounted electrical machinery and including their retail sale.

**Master Plan:** a rendered plan that reflects the character of the overall development and shows the

phasing for the entire tract.

**Meeting Hall:** a building available for gatherings, including conferences. If constructed, the Meeting Hall may be used for the marketing purposes of the development.

**Minor Exception:** are minor departures from the TND Overlay or the approved TND Plan and shall be granted administratively through the Conway Planning Department. A Minor Exception is a ruling that would permit a practice that is not consistent with a specific provision of this TND Overlay or the approved TND Plan but is justified by the Intent (Section 1.2).

**Mixed Use:** multiple functions within the same building through superimposition or adjacency, or in multiple buildings within the same area by adjacency. Mixed use is one of the principles of TND development from which many of its benefits are derived, including compactness, pedestrian activity, and parking space reduction.

**Neighborhood:** an urbanized area at least 40 acres that is primarily Residential. A Neighborhood shall be based upon a partial or entire Standard Pedestrian Shed. The physical center of the Neighborhood should be located at an important traffic intersection associated with a Civic or Commercial institution.

**Net Developable Area, Net Site Area:** the developable areas of a site.

**Office:** premises available for the transaction of general business but excluding retail, artisan and manufacturing uses.

**Outbuilding:** an accessory building, usually located towards the rear of the same lot as a Principal Building. It is sometimes connected to the principal building by a Backbuilding. Outbuildings shall not exceed 600 square feet of habitable space, excluding parking areas.

**Parking Structure:** a building containing two or more stories of parking. Parking Structures shall have Liner Buildings at the first story or higher.

**Passage (PS):** a pedestrian connector passing between buildings, providing shortcuts through long blocks and connecting rear parking areas to frontages. Passages may be roofed over.

**Path (PT):** a pedestrian way traversing a park or rural area, with landscape matching the contiguous open space. Paths should connect directly with the urban sidewalk network.

**Patternbook:** a book showing the Architectural Standards such as building design, style, materials and colors; and Landscape Standards such as trees, plans and layout that the development in the TND Plan must adhere to.

**Pedestrian Shed:** An area, approximately circular, that is centered on a Common Destination. A Pedestrian Shed is applied to determine the approximate size of a Neighborhood. A Standard Pedestrian Shed is 1/4 mile radius or 1320 feet, about the distance of a five-minute walk at a leisurely pace. It has been shown that provided with a pedestrian environment, most people will walk this distance rather than drive. The outline of the shed must be refined according to actual site conditions, particularly along Thoroughfares. The Common Destination should have the present or future capacity to accommodate a T5 Transect Zone for TND and a T6 Zone for RCD. A Long Pedestrian Shed is 1/2 mile radius or 2640 feet, and may be used for mapping when transit is present or proposed. (Sometimes called a "walkshed" or "walkable catchment.") A Linear Pedestrian Shed is elongated to follow a Commercial corridor. See **Standard, Long, or Linear Pedestrian Shed**.

**Planter:** the element of the public streetscape which accommodates street trees. Planters may be continuous or individual.

**Principal Building:** the main building on a lot, usually located toward the frontage.

**Private Club:** an establishment serving alcohol that must be permitted by the Arkansas ABC.

**Private Frontage:** the privately held layer between the frontage line and the principal building facade. The structures and landscaping within the Private Frontage may be held to specific standards. The variables of Private Frontage are the depth of the setback and the combination of architectural elements such as fences, stoops, porches and galleries.

**Public Frontage:** the area between the curb of the vehicular lanes and the Frontage Line. Elements of the Public Frontage include the type of curb, walk, planter, street tree and streetlight.

**Rear Alley (AL):** a vehicular driveway located to the rear of lots providing access to service areas and parking, and containing utility easements. Alleys should be paved from building face to building face, with drainage by inverted crown at the center or with roll curbs at the edges.

**Rear Lane (LA):** a vehicular driveway located to the rear of lots providing access to parking and outbuildings and containing utility easements. Rear lanes may be paved lightly to driveway standards. Its streetscape consists of gravel or landscaped edges, no raised curb and is drained by inverted crown or percolation.

**Rearyard Building:** a building that occupies the full frontage line, leaving the rear of the lot as the sole yard. This is a more urban type, as the continuous facade spatially defines the public thoroughfare. For its residential function, this type yields a rowhouse. For its commercial function, the rear yard can accommodate substantial parking.

**Regulating Plan:** a plan that assigns Transect Zones to the entire TND Overlay District development. This plan identifies the Transect Zones that will govern each tract of land within the TND Overlay District area.

**Residential:** premises available for long-term human dwelling.

**Retail:** premises available for the sale of merchandise and food service.

**Retail Frontage Line:** Frontage Lines designated on a Community Plan that require the provision of a Shopfront, causing the ground level to be available for retail use.

**Road (RD):** a thoroughfare of low vehicular speed and capacity that is located in T3 and T4. Roads are local and suburban thoroughfares with low intensity residential uses. Roads are typically two-way travel and do not usually have parking. The Public Frontage may have curb and gutter or swales; sidewalks, walking paths or bicycle trails; and trees located in planters or natural clusters.

**Rowhouse:** a single-family dwelling that shares a party wall with another of the same type and occupies the full frontage line (Syn: Townhouse; see **Rearyard Building**).

**Rural Boundary Line:** the extent of potential urban growth as determined by existing geographical determinants. The rural boundary is permanent.

**Service Boundary Line:** the extent of potential or feasible urban growth as determined by the extension of infrastructure, principally sewer.

**Setback:** the area of a lot measured from the lot line to a building facade or elevation. This area must be maintained clear of permanent structures with the exception of: galleries, fences, garden walls, arcades, porches, stoops, balconies, bay windows, terraces and decks (that align with the first story level) which are permitted to encroach into the Setback.

**Shared Parking Policy:** an accounting for parking spaces that are available to more than one function. The requirement is reduced by a factor, shown as a calculation. The Shared Parking ratio varies according to multiple functions in close proximity which are unlikely to require the spaces at the same time.

**Sideyard Building:** a building that occupies one side of the lot with a setback to the other side.

**Sidewalk:** the paved layer of the public frontage dedicated exclusively to pedestrian activity.

**Specialized Building:** a building that is not subject to Residential, Commercial, or Lodging classification. Most specialized buildings are dedicated to manufacturing and transportation, and are distorted by the trajectories of machinery.

**Standard Pedestrian Shed:** An area, approximately circular, that is centered on a Common Destination. A Pedestrian Shed is applied to determine the approximate size of a Neighborhood. A Standard Pedestrian Shed is 1/4 mile radius or 1320 feet, about the distance of a five-minute walk at a leisurely pace. It has been shown that provided with a pedestrian environment, most

people will walk this distance rather than drive. The outline of the shed must be refined according to actual site conditions, particularly along thoroughfares. (Sometimes called a “walkshed” or “walkable catchment.”) See **Pedestrian Shed**.

**Story:** a habitable level within a building of no more than 14 feet in height from finished floor to finished ceiling. Attics and raised basements are not considered stories for the purposes of determining building height.

**Street (ST):** a local urban thoroughfare of low speed and capacity that is located in T3, T4 and T5. Streets are local thoroughfares in urban areas with predominately residential uses. Streets may have one or two-way travel, typically with one or two travel lanes and parallel parking. Streets have curb, gutter, sidewalks and trees located in planters.

**Streetscape:** the urban element that establishes the major part of the public realm. The streetscape is composed of thoroughfares (travel lanes for vehicles and bicycles, parking lanes for cars, and sidewalks or paths for pedestrians) as well as the visible private frontages (building facades and elevations, porches, yards, fences, awnings, etc.), and the amenities of the public frontages (street trees and plantings, benches, streetlights, etc.).

**Streetscreen:** sometimes called Streetwall. A freestanding wall built along the frontage line, or coplanar with the facade, often for the purpose of masking a parking lot or storage from the thoroughfare. Streetscreens should be between 3.5 and 8 feet in height and constructed of a material matching the adjacent building façade or may be a hedge or fence. Streetscreens shall have openings no larger than is necessary to allow automobile and pedestrian access. In addition, all streetscreens over 4 feet high should be permeable or articulated to avoid blank walls.

**Substantial Modification:** alterations to a building that are valued at more than 50% of the replacement cost of the entire building, if new.

**Terminated Vista:** a location at the axial conclusion of a thoroughfare. A building located at a Terminated Vista designated on a Community Plan is required to be designed in response to the axis.

**Thoroughfare:** a vehicular way incorporating moving lanes and parking lanes within a right-of-way.

**Traditional Neighborhood Development (TND):** a Community Type based upon a Standard Pedestrian Shed oriented toward a Common Destination consisting of a mixed-use center or corridor, and having a minimum developable area of 40 acres. A TND may be comprised of a partial or entire Standard Pedestrian Shed or more than one Standard Pedestrian Shed. (Syn.: Village, Urban Village).

**Traditional Neighborhood Development (TND) Plan:** a Plan for a Traditional Neighborhood prepared according to this Traditional Neighborhood Development Overlay District Ordinance. A TND Plan consists of two elements: a Master Plan and a Regulating Plan.

**Town Center:** the mixed-use center or main Commercial corridor of a community. A Town Center in a hamlet or small TND may consist of little more than a meeting hall, corner store, and main civic space. A Town Center for RCD or TOD communities may be a substantial downtown Commercial area, often connected to other Town Centers by transit.

**Townhouse:** Syn. Rowhouse. (See **Rearyard Building**.)

**Transect:** a system of ordering human habitats in a range from the most natural to the most urban. The SmartCode is based upon six Transect Zones which describe the physical character of place at any scale, according to the density and intensity of land use and urbanism.

**Transect Zone (T-Zone):** Transect Zones are administratively similar to the land-use zones in conventional codes, except that in addition to the usual building use, density, height, and setback requirements, other elements of the intended habitat are integrated, including those of the private lot and building and the enfronting public streetscape. The elements are determined

by their location on the Transect scale. The T-Zones are: T1 Natural, T2 Rural, T3 Sub-Urban, T4 General Urban, T5 Urban Center, and T6 Urban Core (see Table 1).

**Transition Line:** a horizontal line spanning the full width of a facade, expressed by a material change or by a continuous horizontal articulation such as a cornice or a balcony.

**Type:** a category determined by function, disposition, and configuration, including size or extent. There are community types, street types, civic space types, etc. (See also: **Building Type**.)

**Urban Street (US):** a thoroughfare of low speed and capacity that is located in T5 and T6.

Urban Streets are local thoroughfares in urban centers with more intensive commercial or civic uses. Urban Streets may have one or two-way travel, typically with one or two travel lanes and parallel or angle parking. The Public Frontage has curb, gutter and wider sidewalks. Urban Streets may have trees located in tree wells.

**Work-Live:** a fee-simple mixed-use unit with a substantial Commercial component that may accommodate employees and walk-in trade. (Syn.: Live-With.) (See **Live-Work**.)